

GEORGE W. LILES PKWY SMALL AREA PLAN

ADOPTED | AUGUST 11, 2022



Concord
NORTH CAROLINA

Planning & Neighborhood Development

CITY OF CONCORD MISSION STATEMENT

THE CITY OF CONCORD PARTNERS WITH OUR COMMUNITY TO DELIVER EXCELLENT SERVICE, AND PLANS FOR THE FUTURE WHILE PRESERVING, PROTECTING, AND ENHANCING THE QUALITY OF LIFE.

CORE VALUES

CULTURE OF EXCELLENCE:

We **respect** members of the public and each other, and treat all with courtesy and dignity. We rely on **teamwork** to provide a seamless experience for all customers. We uphold high **ethical standards** in our personal, professional, and organizational conduct. We **continuously improve** by promoting innovation and flexibility to best meet the needs of customers with available resources.

ACCOUNTABILITY:

We accept responsibility for our personal and organizational decisions and actions, while delivering cost-effective and efficient services that are done right the first time.

COMMUNICATION:

We openly communicate with the public and each other by sharing information and soliciting feedback to achieve our goals.

ENVIRONMENT:

We are concerned about our natural, historic, economic, and aesthetic resources and work to preserve and enhance them for future generations.

SAFETY:

We use education, prevention, and enforcement methods to protect life and property in the community, and maintain our infrastructure and facilities to provide a safe environment in which to live, work, shop, and play.

TRUST:

We realize the perception of our organization is dependent upon the public's confidence in our commitment to our core values and to meeting the goals set collectively by the Mayor and City Council.

AN ORDINANCE ADOPTING THE GEORGE W. LILES PARKWAY SMALL AREA PLAN (GLSAP) OF THE CITY OF CONCORD, NORTH CAROLINA

WHEREAS, the City of Concord, North Carolina pursuant to the authority conferred by the North Carolina General Statute §160A-364 enacted an Official Zoning Ordinance for the City of Concord, North Carolina and the Area of Extraterritorial Jurisdiction on July 28, 1977; and

WHEREAS, the City of Concord, North Carolina pursuant to the authority conferred by North Carolina General Statute §§160D-501 shall adopt and maintain a comprehensive plan that sets forth goals, policies, and programs intended to guide the present and future physical, social, and economic development of the jurisdiction; and

WHEREAS, the City may prepare and adopt other such plans as deemed appropriate, which may include small area plans and neighborhood plans; and

WHEREAS, the adopted 2030 Land Use Plan recommends the preparation of a plan for the general area of the George W. Liles Parkway, and the City has obtained property owner and public input in the development of the plan; and

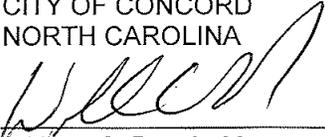
WHEREAS, the Planning and Zoning Commission has unanimously recommended approval of the plan on July 19, 2022.

NOW, THEREFORE, BE IT ORDAINED by the City Council of the City of Concord, North Carolina:

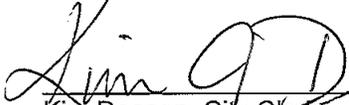
SECTION 1: That the George W. Liles Parkway Small Area Plan as indicated on Attachment A is adopted.

SECTION 2: That this Ordinance be effective immediately upon adoption.

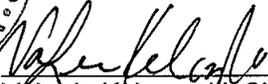
Adopted in this August 11, 2022.

CITY COUNCIL
CITY OF CONCORD
NORTH CAROLINA

William C. Dusch, Mayor

ATTEST:


Kim Deason, City Clerk




Valerie Kolczynski, City Attorney

ACKNOWLEDGMENTS

ELECTED OFFICIALS

William C. "Bill" Dusch, Mayor
Andy Langford, Council Member District 1
W. Brian King, Council Member District 2
Betty M. Stocks, Council Member District 3
JC McKenzie, Council Member District 4
Terry Crawford, Council Member District 5
Jennifer P. Hubbard, Council Member District 6
John Sweat, Jr., Council Member District 7

CITY STAFF

Lloyd Payne, City Manager
Pam Hinson, Asst. City Manager
Steve Osborne, Planning & Neighborhood
Development Director
Kevin Ashley, Planning & Neighborhood
Development Deputy Director
Kaylee Caton, Design Manager
Scott Sherrill, Planning & Development Manager
Joseph Beasley, Economic Dev. Manager
Gerald Warren, Urban Planner
George Daniels, Senior Planner (GIS)

Phillip Graham, Transportation Director
Jeff Corley, Water Resources Director
Alex Burris, Electric Systems Director
Bob Dowless, Parks & Recreation Director
Shelia Lowry, Deputy Parks & Recreation Director
Sue Hyde, Engineering Director

PLANNING & ZONING COMMISSION

John Howard (Chair)
Jean King (Vice Chair)
Maya Jones
Eric Williamson
Philip Jones
Brittany Evans
Deanne Haney (Alternate Member)
Jim Hays (Alternate Member)

MARKET ANALYSIS

Rose & Associates Southeast, Inc.

RESIDENTS OF CITY OF CONCORD

A special thanks goes out to the public who attended the virtual community input sessions; participated in stakeholder meetings; and provided responses to the on-line public input surveys.

TABLE OF CONTENTS

1

INTRODUCTION

- 1.1. Executive Summary
- 1.2. Steps in the Planning Process
- 1.3. How to Use this Plan

2

STUDY AREA A

- 2.1. Area A Plan Overview
- 2.2. Existing Conditions
- 2.3. Future Focus
- 2.4. Area A Recommendations
- 2.5. Implementation

3

STUDY AREA B

- 3.1. Area B Plan Overview
- 3.2. Existing Conditions
- 3.3. Future Focus
- 3.4. Area B Recommendations
- 3.5. Implementation

APPENDICES

- Appendix A: Area A - Lifestyle Development Guidelines
- Appendix B: Area B - Modified Industrial Standards
- Appendix C: Open Space Connectivity Analysis Maps
- Appendix D: Long Range Transit Master Plan Maps

PART 1: INTRODUCTION

1.1. EXECUTIVE SUMMARY

- 1.1.1. Introduction
- 1.1.2. Plan Overview
- 1.1.3. The Vision
- 1.1.4. Market Analysis Summary

1.2. STEPS IN THE PLANNING PROCESS

- 1.2.1. Planning Process
- 1.2.2. Public Input Survey Results

1.3. HOW TO USE THIS PLAN



1.1. EXECUTIVE SUMMARY

1.1.1. INTRODUCTION

The purpose of this Small Area Plan for the George W. Liles Pkwy Corridor is to clearly articulate a vision and define goals for future development and public investment in the study area. This plan will serve as a guiding document to reference for the next 15 to 20 years.

Based on public engagement, market analysis, and substantial input from stakeholders, the following vision was developed for the Study Area:

Future development of the George Liles corridor should include vibrant live-work-play communities with a focus on healthy active living, and connected & accessible places, ultimately enhancing the quality of life of residents and the community.

In support of the Vision, the plan outlines implementation steps from an infrastructure, policy, and regulatory perspective. Specifically, the plan recommends a vision for the following:

**LIVE-WORK-PLAY
COMMUNITY**



**HEALTHY & ACTIVE
LIVING**



**CONNECTED &
ACCESSIBLE PLACES**



**ENHANCED QUALITY
OF LIFE**



THE VISION FOR STUDY AREA A



Live-Work-Play

- Lifestyle-focused development around a village center off George W. Liles Pkwy at the intersection with Concord Farms Road.
- Workplace-focused development near the Concord Parkway/George W. Liles Parkway interchange.
- Create a range of housing opportunities and choices throughout the study area.
- Install the Coddle Creek Greenway & Great Wagon Road Greenway as recommended in the OSCA Plan.



Healthy & Active Living

- Encourage opportunities for urban mini-farms (under five acres) and other agriculture uses to provide local fresh produce.
- Encourage Neighborhoods to develop Community Gardens and programmed Farmer's Markets at the village center.
- Develop a Public Park at the intersection of the two Greenways including restrooms and trailhead parking.
- Install bike facilities, parks and greenways to encourage wellness, active living, and healthy commuting.



Connected & Accessible Places

- Install Public Transit Bus Routes and High Capacity Transit (Bus and/or Light Rail) as recommended in the Long-Range Public Transportation Master Plan.
- Provide safe roads, sidewalks and multi-use paths for all modes of transportation from bus to automobile, bicycle, and foot.
- Re-align and extend Concord Farms Road as recommended to encourage multi-modal connections throughout the study area
- Install clear, well-designed wayfinding and street signage.



Enhanced Quality of Life

- Provide easy access from housing to workplace, dining, shopping and recreational activities via all modes of transportation.
- Provide enhanced design quality that encourages healthy and sustainable design features.
- Create high quality amenities like public art, entertainment, and ample green/open space.
- Develop distinct and attractive communities that have a strong sense of place.

THE VISION FOR STUDY AREA B

Live-Work-Play

- New village center development near the George W. Liles Pkwy/Stough Road and Roberta Road Intersection.
- Encourage small scale industrial workplace opportunities along George W. Liles Pkwy extension near the existing Motorsports Industrial Park.
- Build upon the existing mix of housing opportunities throughout the study area.
- Install the Coddle Creek Greenway as recommended in the Open Space Connectivity Analysis Plan & improve connections to Frank Liske Park.

Healthy & Active Living

- Install bike facilities, parks and greenways to encourage active living.
- Install a multi-use path from Coddle Creek Greenway to Frank Liske Park as recommended in the Open Space Connectivity Analysis.
- Include future expansion plans for Frank Liske Park in the future analysis of Parks & Recreation needs in the surrounding area.

Connected & Accessible Places

- Support NCDOT State Transportation Improvement Program (STIP) project to extend George W. Liles Pkwy all the way to Hwy 49 with a multi-use path on the east side.
- Provide safe roads for all modes of transportation includes bus, automobile, bicycle, and foot.
- Install Public Transit Bus Route connecting Concord and Harrisburg as recommended in the Long-Range Public Transportation Mater Plan.

Enhanced Quality of Life

- Provide easy access from housing to workplace, dining, shopping and recreational activities via automobile, bike and/or foot.
- Ensure compatibility between neighboring land uses and provide adequate buffering between conflicting uses.
- Add more employment opportunities and industries that support The Grounds at Concord.



1.1.2.PLAN OVERVIEW

Established Need

The Implementation Work Plan of the 2030 Land Use Plan, approved in March 2018, calls for a Mixed-Use Corridor Plan for the George W. Liles Parkway corridor from I-85 to US Hwy 29/Concord Parkway S. The City completed a previous Small Area Plan for this corridor, titled "Concord Parkway/Roberta Church Road Small Area Plan", in 2005 before the 2007-09 Great Recession that is now outdated. This update Small Area Plan takes a fresh look at the study area given current market conditions and replaces the 2005 plan. See Page 47 for an overview of the previous Concord Parkway/Roberta Church Road Small Area Plan.

The area surrounding Concord Farms Road and George W. Liles Pkwy is identified in the 2030 Land Use Plan as the Central Concord Character Area. "The Character Areas that are designated on the Growth Concept identify specific areas that, due to existing/expected development pressures, are likely to be focal points for growth and development in Concord through 2030. Each Character Area has different qualities (development pattern, land use mix, etc.) and a role to play in the creation of a more connected and livable Concord." (Source: City of Concord 2030 Land Use Plan) See Map 2-8 on Page 52 for the Growth Concept.

Area Boundaries

The George W. Liles Pkwy Plan is the Small Area Plan for the George W. Liles Pkwy Corridor from Weddington Road to US Hwy 49. Covering 2,418 acres, this plan has been divided into two distinct study areas. See Map 1-1 on Page 11 for the area boundaries.

Area A comprises approximately 1,214 acres located between the Coddle Creek floodplain to the west, George Liles Pkwy to the east, Cannon School to the north, and the Roberta Farms neighborhood to the south. When viewed in the regional context, it is one of the last large, undeveloped areas in the geographic center of Concord. See Map 1-2 on Page 12 for the regional context.

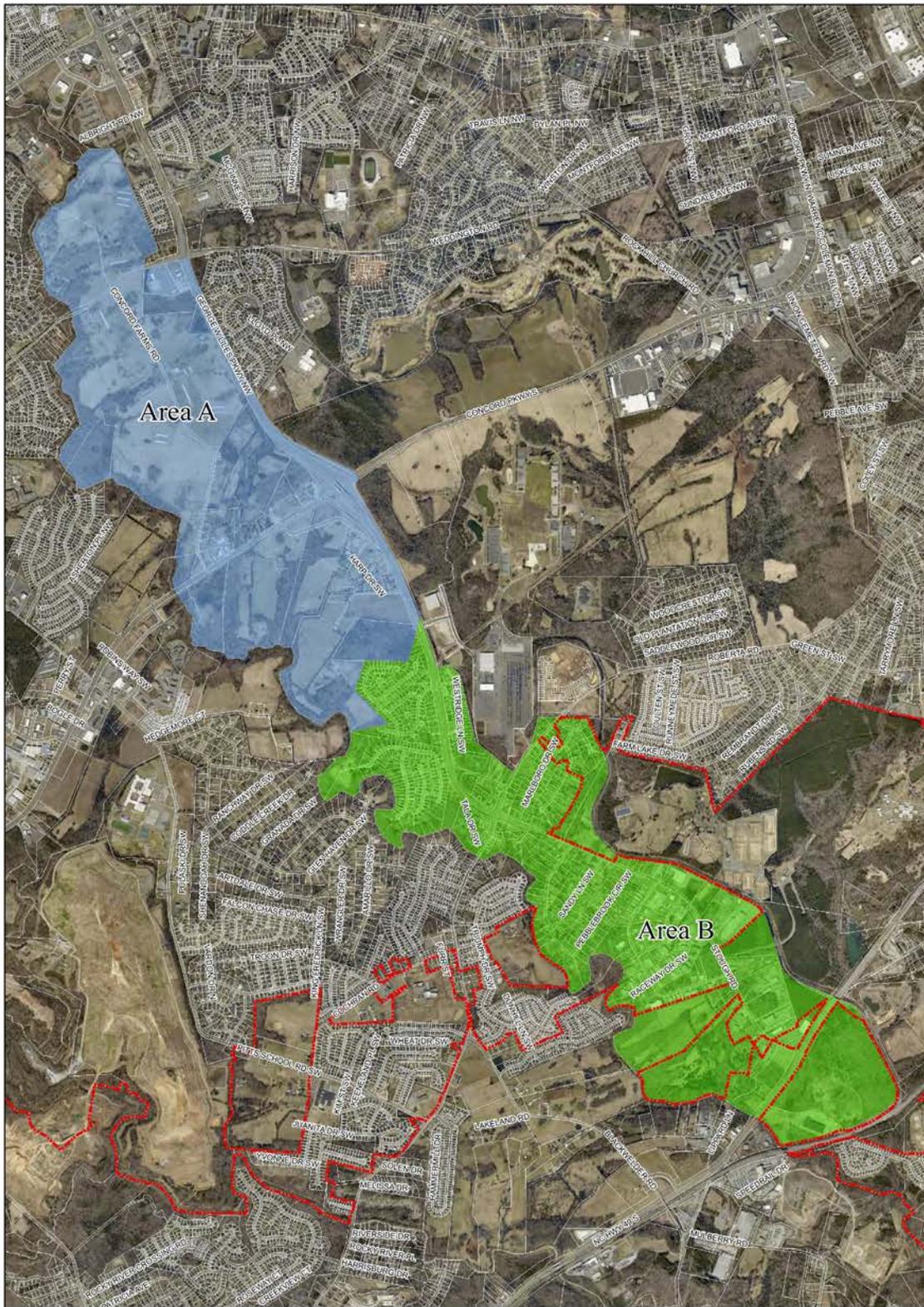


Community Meeting Sign-In | April 28th, 2022

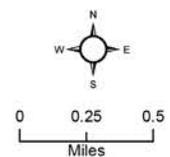
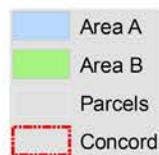


Community Meeting Presentation | April 28th, 2022

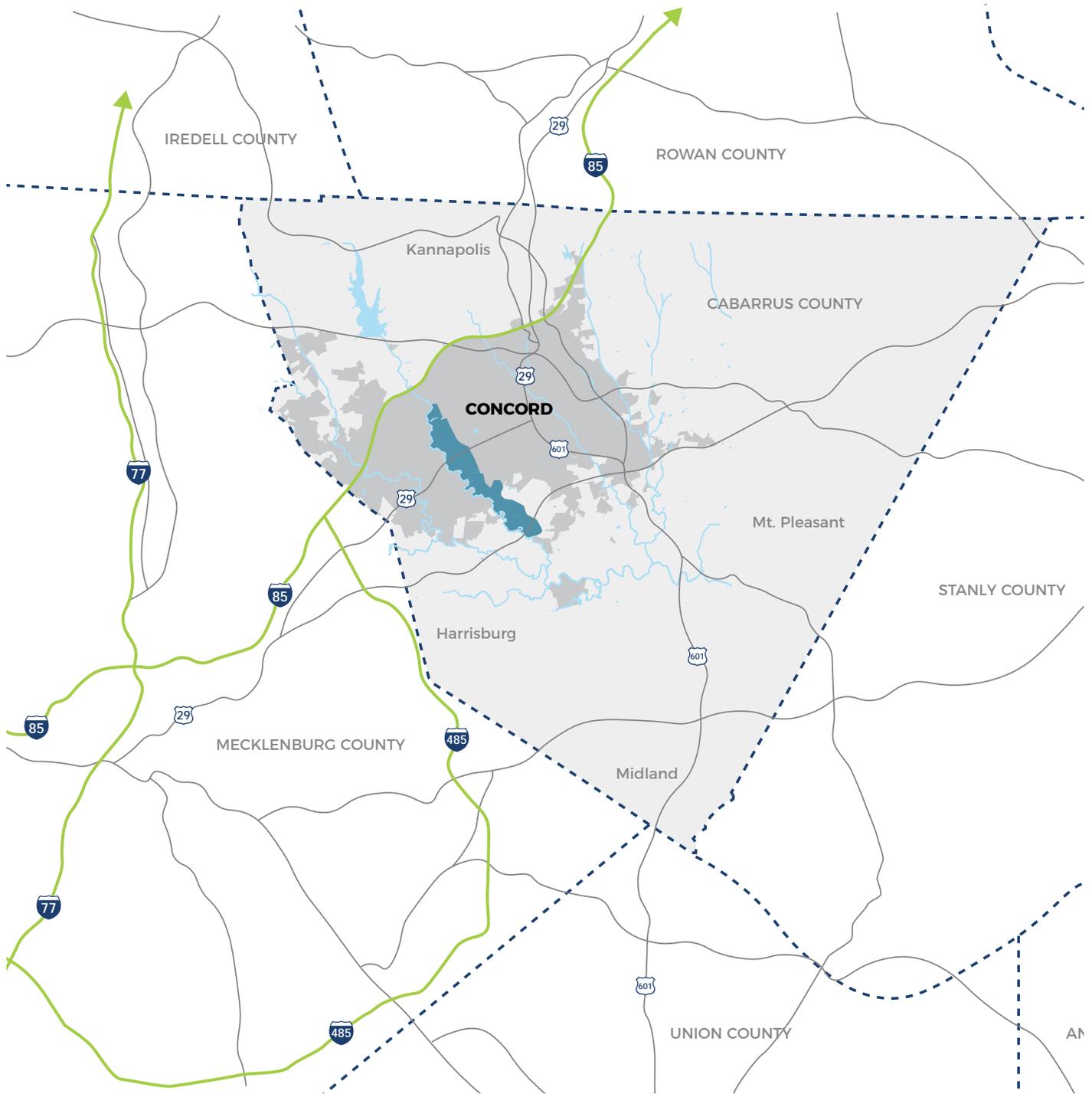
MAP 1-1: GEORGE W. LILES PKWY AREA BOUNDARIES



George W Liles Pkwy
Small Area Plan Boundary



MAP 1-2: REGIONAL CONTEXT



GEORGE W. LILES PKWY SMALL AREA PLAN | REGIONAL CONTEXT

- CABARRUS COUNTY
- CITY OF CONCORD
- SMALL AREA PLAN BOUNDARIES

Area B comprises approximately 1,204 acres located between the Coddle Creek floodplain to the west, the Bootsmead (The Grounds at Concord) Rail Spur to the east, Area A to the north, and some parcels just south of Hwy 49. Unlike Area A, much of Area B is already developed, with a few notable exceptions.

Community Engagement

Following internal stakeholder meetings across City departments (Water Resources, Electric, Transportation, Parks & Recreation and Fire), Rider Transit, Cabarrus County, and the Cabarrus County Economic Development Corporation, staff met with the Key Property owners within the study area to gather input.¹

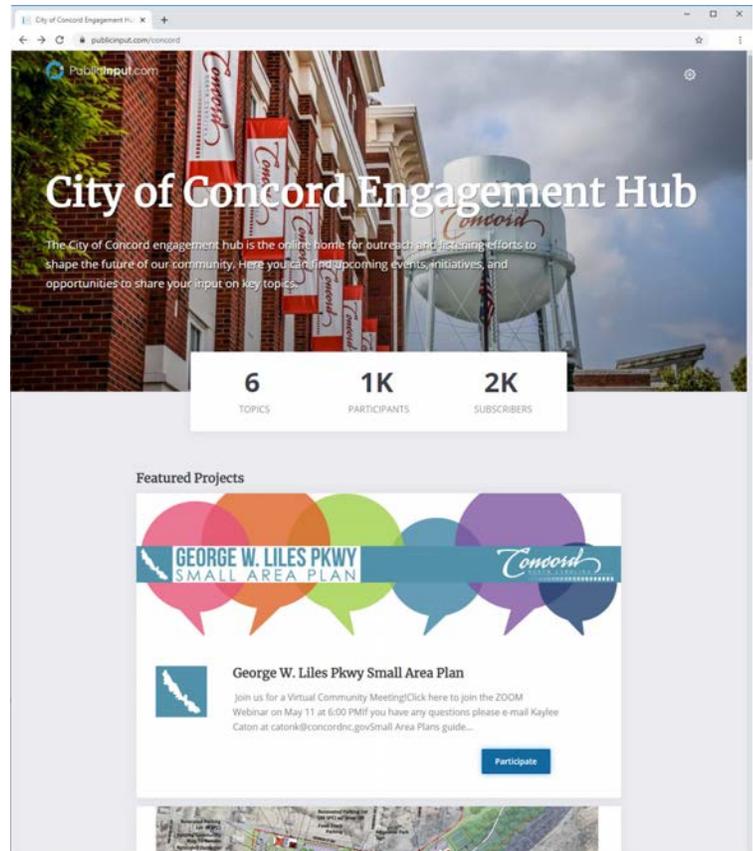
City Staff held thirteen meetings with internal and community stakeholders before COVID-19 restrictions demanded staff to reevaluate how to hold meetings and gather community input. City Staff held ten additional meetings with internal and community stakeholders which were held via the video conferencing platform, Zoom.

In Spring 2020, also due to COVID-19 restrictions, City Staff pivoted from hosting a traditional in-person Open House meeting to a Virtual Community Meeting in order to gather public input for the George W. Liles Parkway Small Area Plan. Overall the Virtual Community Meeting was a success with 246 participants, 5,356 question responses, and 371 comments. The City made a recording of the presentation available on the Public Input Survey webpage for those unable to attend the Virtual Community Meeting in person, which was viewed 329 times. For a summary of the results from the Virtual Community Meeting and Public Input Survey see Pages 28-31.

1.1.3. THE VISION

City Staff identified the vision for the George W. Liles Small Area Plan based on ideas generated through the community engagement process, meetings with internal stakeholders and key property owners, guidance from the Real Estate Market Analysis team, and City Staff input.

The vision defines future aspirations of the Small Area Plan including vibrant live-work-play communities with a focus on healthy active living, connected & accessible places, and an overall enhanced quality of life for residents and the community.



The City's Public Input Engagement Hub was utilized for all project updates as well as a Community Input Survey open from May 11th to June 5th, 2020.

¹For the sake of this Small Area Plan a Key Property owner was defined as those who own more than 100 acres within the study area boundaries.

LIVE-WORK-PLAY COMMUNITY

Live-work play communities are rooted in the idea that people are happier living in neighborhoods that provide for most of their needs. A live-work-play community has a variety of housing, is close to local workplace options where residents of the community work, and provides places for recreation, shopping, eating and entertainment. Live-work-play communities provide benefits such as convenience for low-stress living, more walking, a healthier lifestyle, and greater connection with neighbors.





LIVE-WORK-PLAY COMMUNITY





HEALTHY & ACTIVE LIVING





HEALTHY AND ACTIVE LIVING

Healthy and active living choices, beyond their natural habits, involve the provision of infrastructure and amenities that encourage healthy activity and access to healthy food. In a general study of active living infrastructure, indicators may include miles of bike lanes, sidewalks, greenways and amount of active open space, while access to healthy food may be measured by proximity of a population to grocery stores. These amenities are among the top desired to drive demand for both housing and workspace development and will continue to be important in the post COVID-19 recovery. The George W. Liles Pkwy could be uniquely positioned as a place that is driven by healthy choices in exercise, food, environment, and sustainability. Community gardens, farm to table restaurants and urban mini farms could anchor key catalyst lifestyle master planned developments and tie into the history of the area.

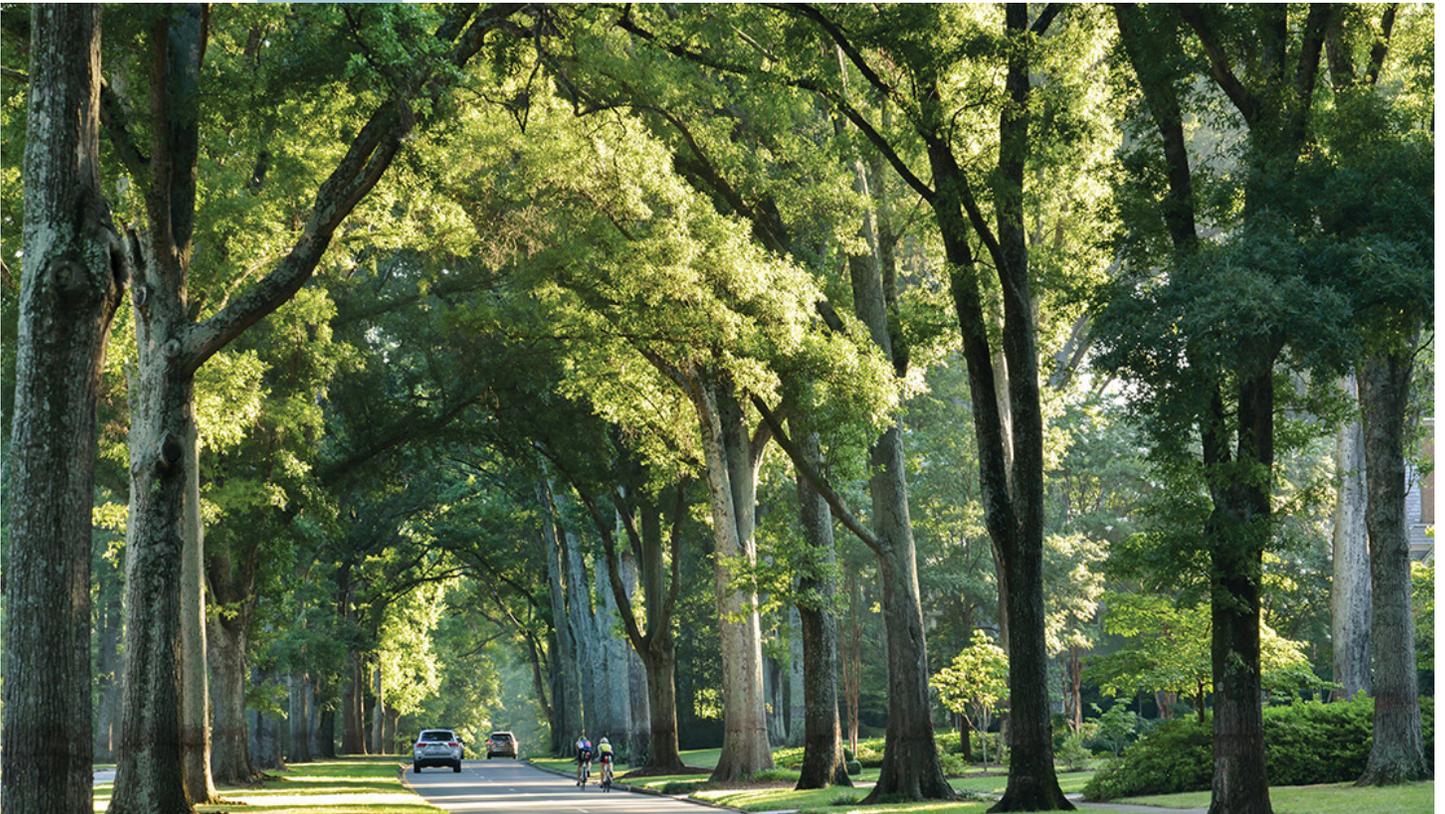
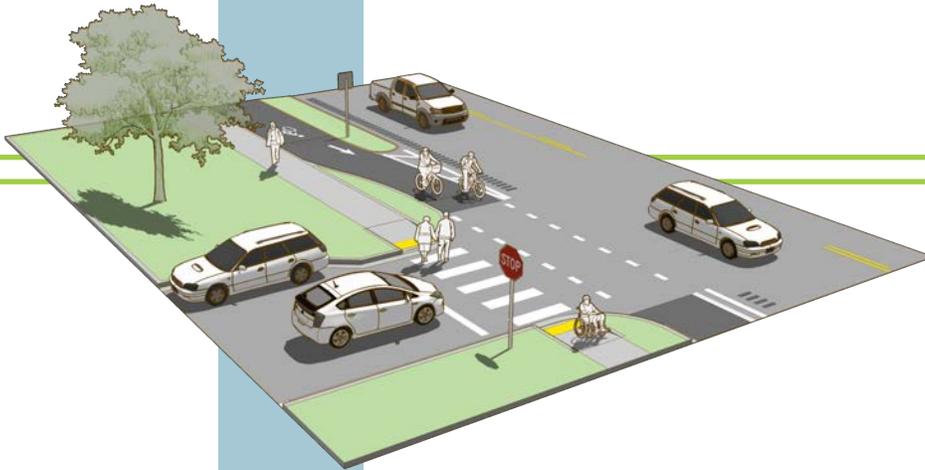


CONNECTED & ACCESSIBLE PLACES



CONNECTED & ACCESSIBLE PLACES

A key feature of connected and accessible places is a multi-modal transportation network that links the land uses along the corridor and accounts for all users in a way that minimizes conflict. For this small area plan, the ideal transportation network will provide access from the neighborhood to employment and lifestyle centers and from employment and industrial centers to major transportation routes. Connected and accessible places create an intuitive, safe network that is attractive to users of all ages and abilities. This infrastructure attracts businesses and employers because of the high quality of life and sense of place that ample connectivity can provide.





ENHANCED QUALITY OF LIFE

Local community culture and quality of life are both drivers for workforce development and include things like housing choices, transportation options, proximity to parks/greenways, and places for dining, shopping and entertainment based on connectivity and experience. This Small Area Plan aims to layout growth and future development along the George W. Liles Parkway in such a manner as to enhance the community's quality of life and position the George W. Liles Pkwy Corridor as a place people want to live, work and visit.





ENHANCED QUALITY OF LIFE



1.1.4. MARKET ANALYSIS SUMMARY

A market analysis for the George Liles Parkway Small Area Plan was completed by Rose Associates in early 2020. The full report can be viewed as an attachment to this plan. The analysis looks at the demographics and economics of Concord and the Plan area, as well as the market dynamics that impact land use.

Community Snapshot

Highlights of the 2019 demographic and economic data for the City of Concord are shown here. Concord is the largest city in Cabarrus County and is the County Seat. The data reveals an educated middle-class population, the majority of which (66%) are employed in white collar professions.

From 2010 (the date of the last US Census) to 2019 estimates, the City grew in population by 24.1%, with growth rates projected 1.91% annually through 2024. Approximately 61% of the population are from generations of working age adults (18-64 years old).

By contrast, the Study Area with an estimated 2019 population of 2,822, reveals an older more affluent district with higher incomes. As many as 75% of workers are employed in white collar professions. This area grew faster than the City or County, from 1,848 in 2010 to 2,822 in 2019.

Market Dynamics

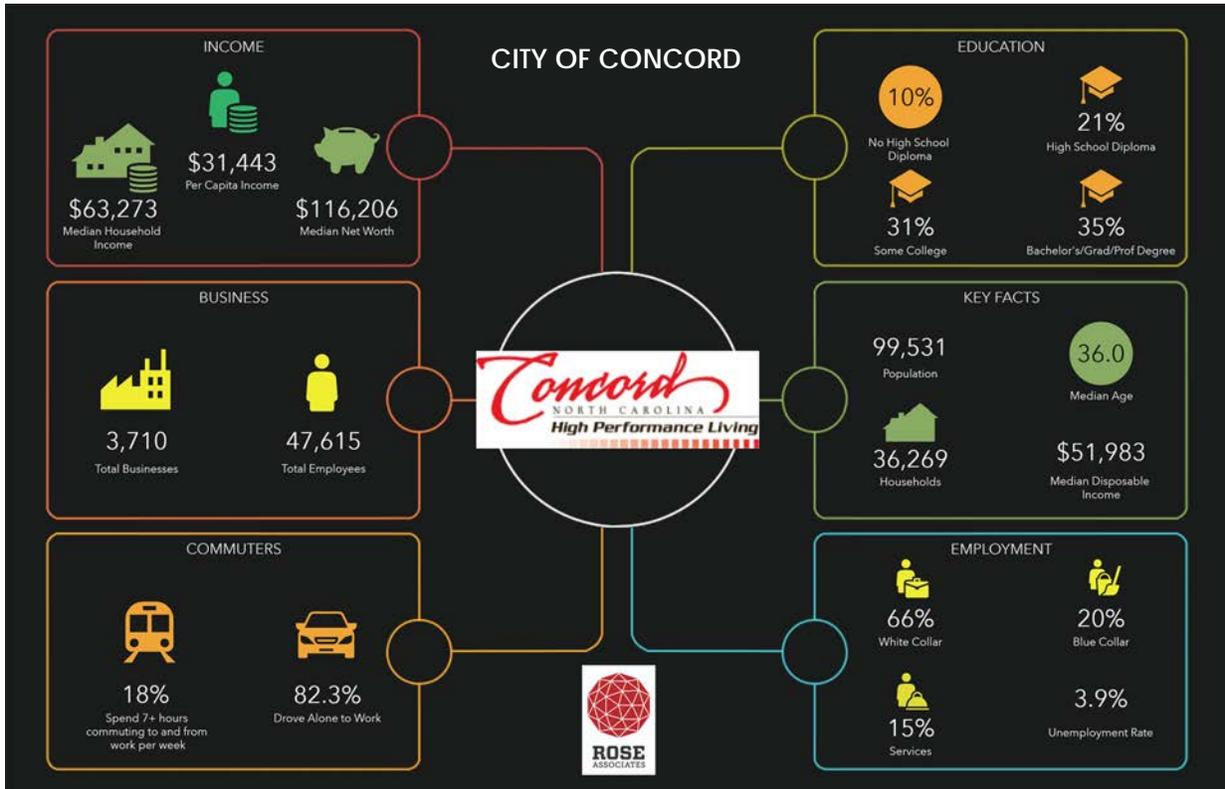
Changing Markets

Market Demand has traditionally been driven by workforce development and job growth. However, these traditional norms have been disrupted by a new generation of workers who choose the community in which they wish to live based on local community culture and quality of life. These include housing choices, transportation options, proximity to parks/greenways, and places for dining, shopping and entertainment based on connectivity and experience. Other disrupters such as co-working spaces (e.g. WeWork) have also changed the work environment. Additionally, artificial intelligence and technology have changed the way real estate is utilized. Many of the trends that were already shifting land use and product types were accelerated with the Covid-19 pandemic. Our perspective on living and work environments changed within weeks. The plan for the George Liles Parkway takes the long view and an opportunity to address the current market, but more importantly, a plan for the future to create a market for products and offerings not currently being offered in the community.

Cabarrus County

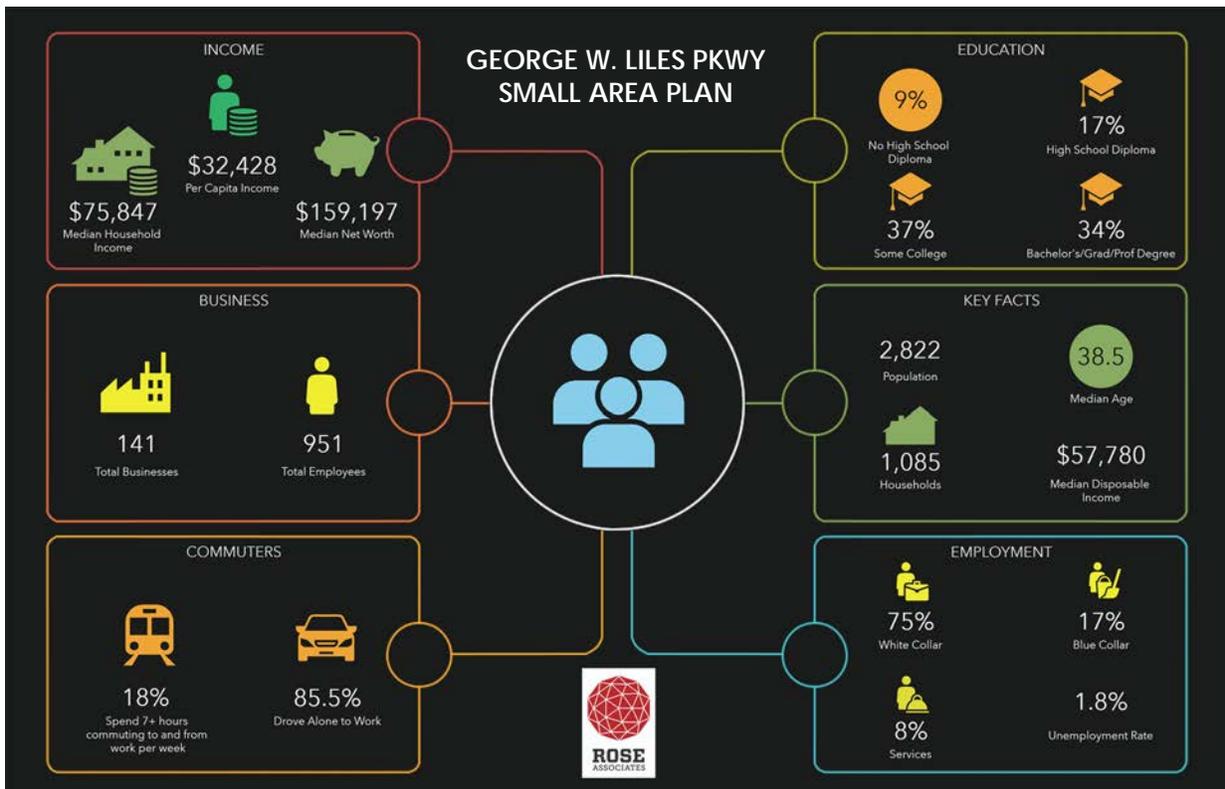
The economy in Cabarrus County is driven primarily by two dynamics: Tourism and Manufacturing/Logistics. The major industry sectors within each of these result in an economy that serves primarily visitors and consumers from outside the region. The strategies, resources, and incentives for growth in the County have been focused in these two areas. However, there may be an opportunity to further diversify the economy to attract a new generation of workers and targeted industries that can strengthen its downtowns and create opportunities for bold vision and master planned communities that attract the lifestyle environments that appeal to both residents and leading edge companies.

FIGURE 1-1: CITY OF CONCORD - COMMUNITY SNAPSHOT



Concord City 2019 Estimates - Source: ESRI, Rose Associates

FIGURE 1-2: GEORGE W. LILES PKWY - COMMUNITY SNAPSHOT



Study Area 2019 Estimates - Source: ESRI, Rose Associates

George Liles Parkway

The Parkway is strategically located between Interstate 85 to the north and NC Highway 49 to the south and lies between the University of North Carolina Charlotte (Mecklenburg County) and the NC Research Campus (Kannapolis). The primary influencing factor impacting the Parkway is the former Phillip Morris campus, now under redevelopment as The Grounds at Concord (The Grounds), a 1,900 acre project that includes a 700+ acre “Super Site” for manufacturing. Two new very large facilities were recently announced including Eli Lilly and Red Bull, Rouch & Ball Partnership. This is in addition to the Carvana inspection and reconditioning facility and Golden Home International’s new North American Headquarters for this leading residential cabinet maker. While The Grounds provides continued options for competitive manufacturing, the unique features along the Parkway also provide an opportunity for creating placemaking through master planned development with a focus on both lifestyle and employment for a new generation.

Key themes in the Real Estate and Land Use Strategy include the following:

Lifestyle Options

Feedback from stakeholders and a review of the housing market suggest that a key component missing from the local market includes a master planned community with lifestyle amenities such as connected greenways, public gathering space and historical relevance (e.g. Great Wagon Trail, Coddle Creek and other historical/agricultural elements). This would include housing of all price ranges from affordable to executive housing for working age generations and retirees, both consumer markets that are seeking similar experiences.

Workspace Options

The majority of commercial product inventory in the region is retail and industrial. The Cabarrus County Submarket is among the top performers within the Charlotte region in these product types. However, less than 5% of the office market for the Charlotte region is located within Cabarrus County. Concord’s position as the county seat, adjacent to major educational and health institutions suggest a strategy to attract both small and larger office investment – in tandem with targeted industries identified with local/regional Economic Development organizations. Master planned campuses focused on employment must include unique amenities to attract future forward companies and their discerning employees.

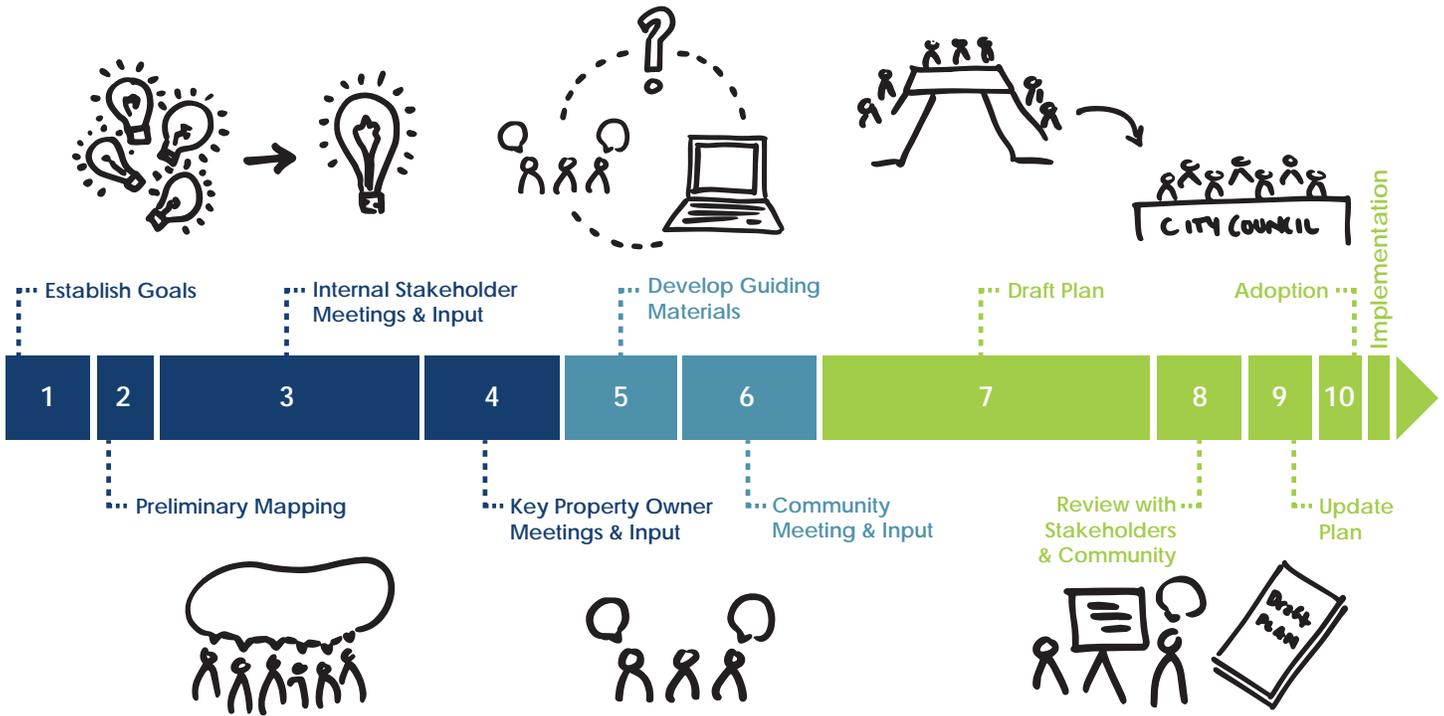
Healthy Community Focus

Both consumers and employers seek new lifestyle and workspace options that include access to open space, greenways and dining. These amenities are among the top desired to drive demand for both housing and workspace and will continue to be critical in the post pandemic recovery period. This was confirmed during interviews and public surveys for the George Liles Parkway plan. Partnerships with landowners, education and health institutions, city/county government and other regional economic development organizations could uniquely position the Parkway as a place that is driven by healthy choices in exercise, food, environment, and sustainability. Community gardens, farm to table restaurants and urban mini farms could anchor key catalyst lifestyle master planned developments. These amenities would be attractive to both residents and employers, through a planned employment campus that strengthens and connects both The Grounds and downtown as companies seek communities and sites that would attract a young, vibrant, active, and educated workforce.

FIGURE 1-3: REAL ESTATE AND LAND USE



FIGURE 1-4: STEPS IN THE PLANNING PROCESS



1.2. STEPS IN THE PLANNING PROCESS

1.2.1. PLANNING PROCESS

Step One: Establish Goals

The first step in the planning process is to establish the boundaries of the Small Area Plan, form the project team, and identify both the internal and external stakeholders and key property owners. During this step, City Staff establishes the overall goals for the Small Area Plan.

Step Two: Preliminary Mapping & Research

The next step in the planning process is to gain an understanding of the area by doing research and learning about its history, demographics, and the built environment. The project team conducts data and map-based analysis and for this Area Plan contracted with Rose & Associates to complete a Real Estate Market Analysis for the study area boundary.

Step Three: Internal Stakeholder Meetings & Input

Once the background mapping and research are complete, the project team puts together presentations and meets with internal stakeholders including City Departmental Staff (Water Resources, Electric, Transportation, Parks & Recreation, Fire, etc.), Rider Transit, Cabarrus County, and the Cabarrus County Economic Development Corporation to gather feedback and insight about the study area.

Step Four: Key Property Owner Meetings & Input

Following meetings with internal stakeholders the project team meets individually with external stakeholders and key property owners. City Staff presents a snapshot of the materials gathered so far and the stakeholders and key property owners provide personal feedback and insight about the study area. City Staff continues meeting with the key property owners multiple times throughout the planning process.

Step Five: Develop Guiding Materials

Taking all of the information gathered in step one through four from mapping, research and various meetings with stakeholders and key property owners, the project team works on public outreach and holds a community meeting. Behind the scenes, staff also worked on developing the guiding materials for the Small Area Plan taking into consideration existing conditions, infrastructure, and the feedback received from internal and key property owner meetings. This material is presented to the public during a community meeting where their feedback is requested.

In the case of this Small Area Plan a Virtual Community Meeting in May 2020 to kick-off the public engagement process of the plan. (In person meetings were moved to virtual meetings in response to COVID-19)

Step Six: Community Engagement & Outreach

Following the Virtual Community Meeting presentation, City Staff made a Public Input Survey available from May 14th through June 5th. Overall, the Virtual Community Meeting was a success with 246 participants, 5,356 question responses, and 371 comments. City Staff made a recording of the presentation available on the Public Input Survey webpage for those unable to attend the Virtual Community Meeting in person, which was viewed 329 times.

Step Seven, Eight, Nine: Draft, Review & Update Plan

Step seven is the task of assembling all of the draft materials that have been produced and writing new material, as needed, to create a complete plan draft. That draft is reviewed by the public, and subsequently refined to create a near-final draft for use in the adoption process.

Step Ten: Adoption

The final step before Implementation takes the draft plan through the city's adoption process. The draft may continue to change as the plan advances through this process and Planning & Zoning Commission and City Council conduct their review and provide additional input and guidance. In conducting their review, Planning & Zoning Commission and City Council evaluate the draft plan based on three criteria: consistency with the 2030 Land Use Plan, inclusive public engagement process, and long-range view.

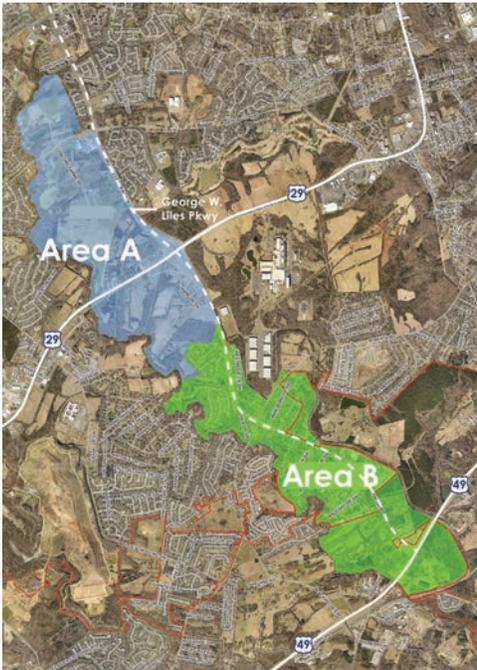
Implementation

Plan implementation begins after the plan has been adopted, and continues indefinitely until the plan vision has been achieved, or until such time that a new plan for the area is undertaken and adopted. The task of implementing the plan is made easier by following the strategy that is outlined in the implementation chapter of the plan. This chapter identifies the relative priority for all of the recommendations in the plan.

1.2.2. PUBLIC INPUT SURVEY RESULTS

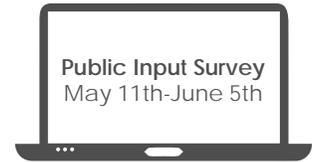
Project Engagement

VIEWS	PARTICIPANTS	RESPONSES	COMMENTS	SUBSCRIBERS
1,601	246	5,356	371	128



80% Reside in Concord, NC

35% Reside in the Study Area



What services would you like to see in Area A?

- 1
- Recreation
- 2
- Dining
- 3
- Entertainment

What services would you like to see in Area B?

- 1
- Recreation
- 2
- Dining
- 3
- Workplace/Employment

What kind of Shopping Experience do you prefer?

- 1
- Main Street
- 2
- Outdoor Mall
- 3
- Specialty Shops

If family/friends/business associates are visiting you in Concord, where do you go with them for shopping, dining and entertainment?

	Shopping	Dining	Entertainment
Concord Mills	61% ★	24%	16%
Downtown Concord	8%	74% ★	17%
Afton Village	24%	69% ★	7%
Charlotte	15%	33%	52% ★
Huntersville/Cornelius	38%	48%	14%
Harrisburg	18%	70% ★	12%
Kannapolis	22%	27%	51% ★
Other	27%	38%	35%

Other: Birkdale, Blakeney, Belmont, Whitewater Center, Salisbury, Greensboro, Raleigh, Other Areas in Concord, UNCC University Area, Winston-Salem

If family/friends/business associates are visiting you in Concord, where do you go with them for recreation?



Frank Liske Park



Dorton Park



Other



Concord Greenways

What is your Vision for Area A?



Sports & Recreation



Shopping & Dining



Lifestyle (Mixed-Use)



Entertainment

What is your Vision for Area B?



Sports & Recreation



Lifestyle (Mixed-Use)



Shopping & Dining



Employment Campus

Define your Vision:

STUDY AREA A:

- Green Space/Leave it Natural
- No Apartments or High Density Housing
- More Parks & Recreation
- More Shopping & Dining
- Mixed-Use Development

STUDY AREA B:

- Green Space/Open Space
- No Apartments
- Stay-As-Is/Limit New Development
- More Shopping & Dining
- Corporate Office/Employment

*Reflects some (not all) of the comments received.

Which architectural style do you prefer?



◀ **BAXTER TOWN CENTER & VILLAGE**
Fort Mill, South Carolina
Main Street Town Center
Walkable Mixed-Use Community

▼ **SERENBE**
Chattahoochee Hills, Georgia
Arts & Crafts Style Architecture
Agriculture, Arts & Health Focus



WANT

- Green Space/Open Space
- Parks, Recreation and Greenways
- Pedestrian & Bike Friendly
- Walkable Community
- Entertainment Options
- More Shopping & Dining Options
- Mixed-Use Development
- Employment Opportunities
- Boutique/Specialty Shops
- Mom-and-Pop Dining Options
- Small Town Character/Feel
- Controlled Development

DON'T WANT

- Apartments
- Increased Traffic Congestion
- Strip Mall Shopping
- Car-Dependent Development
- Industrial Development
(north of Hwy 29)
- High Density Development

*Reflects some (not all) of the comments received for both Area A & B.

Additional Input

STUDY AREA A

"It would be great if a developer like Sun City which builds 55+ communities would come into and develop a community within the Liles Pkwy corridor. The Philip Morris property on Hwy. 29 that backs up to the Cabarrus County Club would be ideal. Concord is seriously lacking 55+ communities! We live in Afton Village and would definitely move to that type of community if it were in the proper location with a good road unlike Poplar Tent Rd."

"A natural area with wooded trails and fields of wildflowers is needed, wanted, and would get the most use. There are too many small shopping/dining venues in this area and none of it is very good. When we moved here all of the natural spaces were a key highlight of this section. Building it up will only continue to detract from the appeal of Concord as a suburb of Charlotte that has a real small town flavor. There really is no need for more apartment buildings or strip malls."

"I am a recreational cyclist. I feel that Concord is lagging behind other communities in making a safe environment for cycling (greenways, trails, bike lanes, etc.) I use the George Liles greenway, but it connects to nothing and has now for years. I would like to see more priority put on a network of connecting greenways. I [do not want to] see any more of these big concrete walled industrial buildings like over near the airport and now on Pitts School road."

"I like the trail alongside George Liles Pkwy [and] would like to see some preservation of outdoor recreation and keep the 'farm-feel' of that walk, or at a minimum some way to tribute the farm origin. Would love to see more unique dining options since chain restaurants more typically line 85, this location would be more for locals to spread the word about 'hidden gems'. Mixed-use would be great, as long as adequate parking is available since that area isn't walkable from many current neighborhoods."

"Please no more apartments! The farm land in this area is naturally beautiful. Would love to see parks and or some mixed use development that has charm and walkable areas."

STUDY AREA B

"[First] major change needs to be working on Stough Road. It cannot handle the current traffic let alone traffic once it is developed. Traffic is too heavy for current traffic flow."

"With it being closer to 49, I see that as more of an opportunity for employment facility & health-care to sustain population growth of this area. Possibly a strip mall set up around it to provide necessary dining & shopping needs that office-working employment centers need."

"Extend walkway/bike path all the way to Frank Liske park along George Liles/Stough Rd or along Wolf Meadow Branch that runs from the narrow bridge on Stough Rd to the branch that empties the lake at the park. Synchronize stoplights at Stough-Roberta intersection with lights at Weddington to allow adequate breaks in traffic for safe entry to Liles Pkwy from neighborhoods without u-turns like Cedar Springs. Certain times of day the wait can be over 5 minutes to make a left turn out of the Cedar Springs neighborhood. This will only get worse as development increases in the area and when Stough Rd. becomes four-lane to 49."

*Reflects some (not all) of the comments received for both Area A & B.

1.3. HOW TO USE THIS PLAN

The recommendations identified in this plan provide direction to guide day-to-day decision making related to land use, public investment, private development, and partnerships. The plan gives the flexibility needed to pursue unforeseen opportunities that will arise and to respond to new challenges over the coming years. The George W. Liles Pkwy Small Area Plan is divided into two Area Boundaries, Area A and Area B (as seen in Map 1-1: George W. Liles Parkway Area Boundaries).

Area Plan Overview

The beginning of each chapter outlines an overview of the study area boundaries, existing conditions, future focus, and area recommendations. This section serves as an introduction and snapshot look at each of the two study areas.

Existing Conditions

This section provides an overview of the existing conditions for each Area Boundary including existing development, public infrastructure, zoning districts and future land use categories.

Future Focus

This section reviews future development that is applicable across each area boundary, divided into four topics:

- Land Use & Built Form
- Transportation
- Public Transit
- Parks & Recreation

Within each topic, the plan describes the vision and key recommendations. Topic sections also include maps, showing where relevant concepts could be located in the area.

Area Recommendations

This section summarizes area recommendations and expands into further detail how each recommendation could be achieved.

Implementation

The implementation section describes the tools and partnerships that will be used in achieving the vision, identifies priority recommendations, and describes how implementation progress will be measured.

Applying the George W. Liles Pkwy Small Area Plan to Rezoning

The George W. Liles Pkwy Small Area Plan, as an adopted city plan, will play an important role in guiding rezoning decisions. Future rezoning must be consistent with the neighborhood context, place, height, and street type designations in this plan, along with the other recommendations.

All rezonings will need to go through the public notice and approval process as this document does not have the authority to rezone. The zoning recommendations in this plan are intended to be implemented legislatively through applicant-driven rezonings: these will be evaluated in light of this adopted plan as well as the 2030 Land Use Plan.

In addition, many of the zoning recommendations anticipate public investment in infrastructure improvements, and these investments may also be a factor in the consideration of rezoning requests. Two 2030 Future Land Use Plan map amendments are anticipated as a result of this planning process.

PART 2: STUDY AREA A

2.1. AREA A PLAN OVERVIEW

2.2. EXISTING CONDITIONS

- 2.2.1. Existing Development
- 2.2.2. Existing Infrastructure
- 2.2.3. Concord Parkway/Roberta Church Rd Small Area Plan
- 2.2.4. Existing Zoning
- 2.2.5. 2030 Land Use Plan

2.3. FUTURE FOCUS

- 2.3.1. Land Use & Built Form
- 2.3.2. Transportation
- 2.3.3. Public Transit
- 2.3.4. Parks & Recreation

2.4. AREA A RECOMMENDATIONS

2.5. IMPLEMENTATION

- 2.5.1. Types of Implementation
- 2.5.2. Key Variables and Constraints
- 2.5.3. Land Use Plan Updates

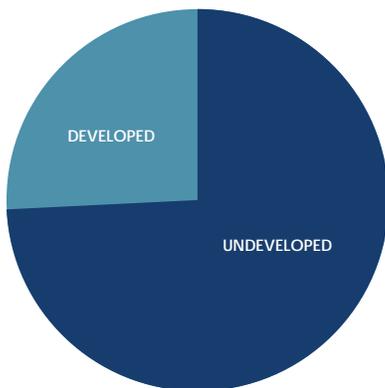
Area A

2.1. AREA A PLAN OVERVIEW

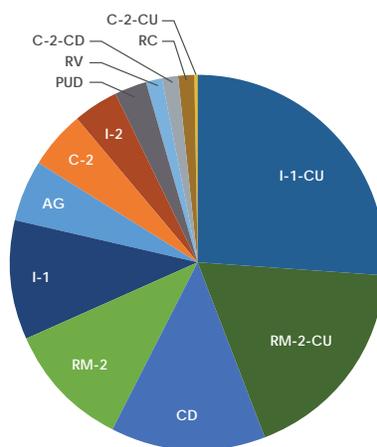
Live-Work-Play

Live-work play communities are rooted in the idea that people are happier living in neighborhoods that provide for most of their needs. A live-work-play community has a variety of housing, is close to local workplace options where residents of the community work, and provides places for recreation, shopping, eating and entertainment. Live-work-play communities provide benefits such as convenience for low-stress living, more walking, a healthier lifestyle, and greater connection with neighbors.

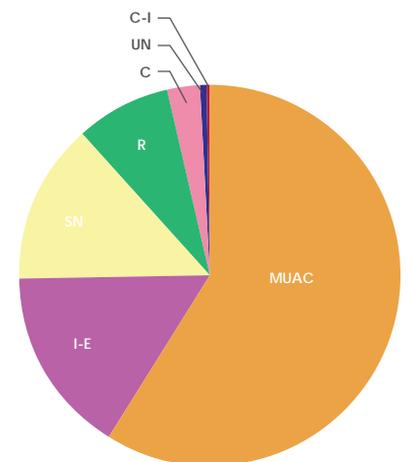
- Lifestyle-focused development around a village center off George W. Liles Pkwy at the intersection with Concord Farms Road.
- Workplace-focused development near the Concord Parkway/George W. Liles Parkway interchange.
- Create a range of housing opportunities and choices throughout the study area.
- Install the Coddle Creek Greenway & Great Wagon Road Greenway as recommended in the OSCA Plan.



Existing Development



Zoning Districts



Future Land Use

Healthy & Active Living

Healthy and active living choices, beyond their natural habits, involve the provision of infrastructure and amenities that encourage healthy activity and access to healthy food. In a general study of active living infrastructure, indicators may include miles of bike lanes, sidewalks, greenways and amount of active open space, while access to healthy food may be measured by proximity of a population to grocery stores. These amenities are among the top desired to drive demand for both housing and workspace development and will continue to be important in the post COVID-19 recovery. The George W. Liles Pkwy could be uniquely positioned as a place that is driven by healthy choices in exercise, food, environment, and sustainability. Community gardens, farm to table restaurants and urban mini farms could anchor key catalyst lifestyle master planned developments and tie into the history of the area.

- Encourage opportunities for urban mini-farms (under five acres) and other agriculture uses to provide local fresh produce.
- Encourage Neighborhoods to develop Community Gardens and programmed Farmer's Markets at the village center.
- Develop a Public Park at the intersection of the two Greenways including restrooms and trailhead parking.
- Install bike facilities, parks and greenways to encourage wellness, active living, and healthy commuting.

Connected & Accessible Places

A key feature of connected and accessible places is a multi-modal transportation network that links the land uses along the corridor and accounts for all users in a way that minimizes conflict. For this small area plan, the ideal transportation network will provide access from the neighborhood to employment and lifestyle centers and from employment and industrial centers to major transportation routes. Connected and accessible places create an intuitive, safe network that is attractive to users of all ages and abilities. This infrastructure attracts businesses and employers because of the high quality of life and sense of place that ample connectivity can provide.

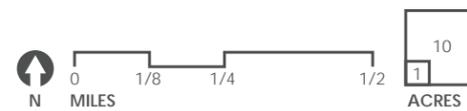
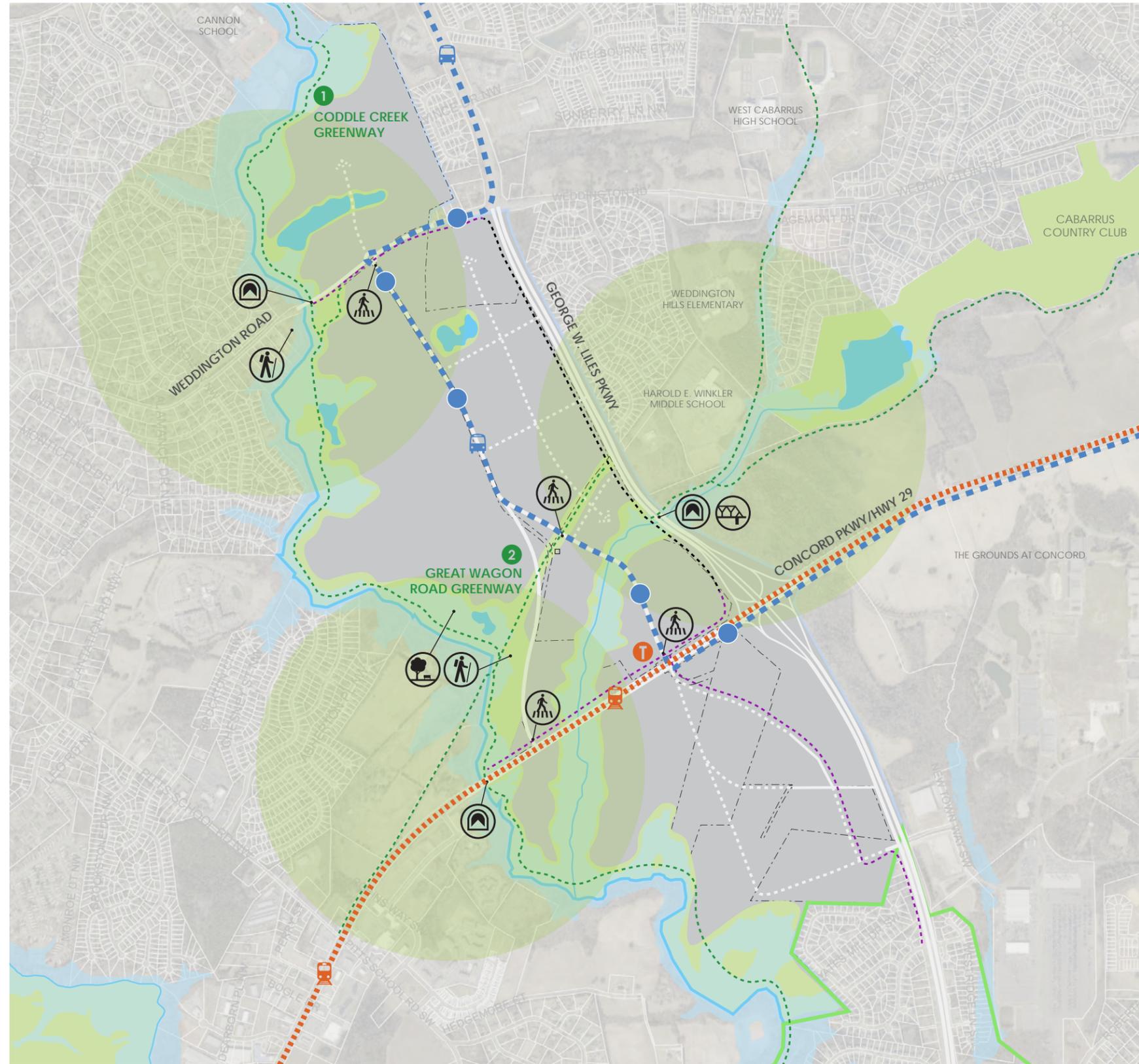
- Install Public Transit Bus Routes and High Capacity Transit (Bus and/or Light Rail) as recommended in the Long-Range Public Transportation Master Plan.
- Provide safe roads, sidewalks and multi-use paths for all modes of transportation from bus to automobile, bicycle, and foot.
- Re-align and extend Concord Farms Road as recommended to encourage multi-modal connections throughout the study area
- Install clear, well-designed wayfinding and street signage.

Enhanced Quality of Life

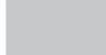
Local community culture and quality of life are both drivers for workforce development and include things like housing choices, transportation options, proximity to parks/greenways, and places for dining, shopping and entertainment based on connectivity and experience. This Small Area Plan aims to layout growth and future development along the George W. Liles Parkway in such a manner as to enhance the community's quality of life and position the George W. Liles Pkwy Corridor as a place people want to live, work and visit.

- Provide easy access from housing to workplace, dining, shopping and recreational activities via all modes of transportation.
- Provide enhanced design quality that encourages healthy and sustainable design features.
- Create high quality amenities like public art, entertainment, and ample green/open space.
- Develop distinct and attractive communities that have a strong sense of place.

MAP 2-1: STUDY AREA A OVERVIEW MAP



MAP LEGEND

-  Study Area
-  Green Space
-  Water
-  100 Yr Floodplain
-  Proposed Greenways
-  Existing Multi-Use Path
-  Proposed Multi-Use Path
-  Greenway
-  Crosswalk
-  Potential Public Park
-  Trailhead Parking
-  Pedestrian Tunnel
-  Pedestrian Bridge
-  1/2 Mile Open Space Search Radius
-  Proposed Light Rail/BRT
-  Proposed Bus Route
-  Proposed Transit Stop
-  Proposed Bus Stop

2.2. EXISTING CONDITIONS

Area A comprises approximately 1,214 acres located between the Coddle Creek floodplain to the west, George Liles Pkwy to the east, Cannon School to the north, and the Roberta Farms neighborhood.

The area is largely undeveloped, but centrally located in Concord as a whole. Development patterns will be largely driven by what occurs in the surrounding area. There is a small existing commercial node at the intersection of Weddington Rd. and George W Liles Pkwy NW, and a more substantial one at the intersection of George W. Liles Pkwy and Poplar Tent Rd., which is less than half a mile by direct route from the northern end of Study Area A.

The western boundary of Study Area A from Cannon School to Hwy 29 generally is separated by Coddle Creek from existing neighborhoods of a variety of intensities that include Kings Crossing, Carriage Downs, the Woodlands, and Covington. Taylor Glen, a retirement community is also across Coddle Creek from the Study Area. Near 29, adjoining development becomes more rural and consists of an existing vineyard and farmland.

South of Hwy 29, the western edge of the study area is bounded by the northern elements of the Settlers Landing mixed use node, and then is separated by a strip of agricultural land from the established neighborhood of Carolando.

Across George Liles Pkwy is an educational hub with an elementary school, middle school, and high school, and the established neighborhood of Sheffield Manor. Just north of Hwy 29, the Grounds at Concord site begins with approximately 401 acres, which expands for an additional 1,480 acres south of Hwy 29 including about 86 acres in Study Area A. All land included in The Grounds property east of George W Liles Pkwy is excluded from the Study Area, but has substantial bearing on how the study area develops.

2.2.1. EXISTING DEVELOPMENT

Propst Farm

The largest portion of the site, at about 550 acres, is owned by the Propst family and is operated for limited farming and agricultural activities. The property is gently rolling towards the creek, and is mostly characterized by its openness. Though there are a number of rock outcroppings, there are only a few significant stands of trees and three watering ponds.

Willow Oaks Crossing

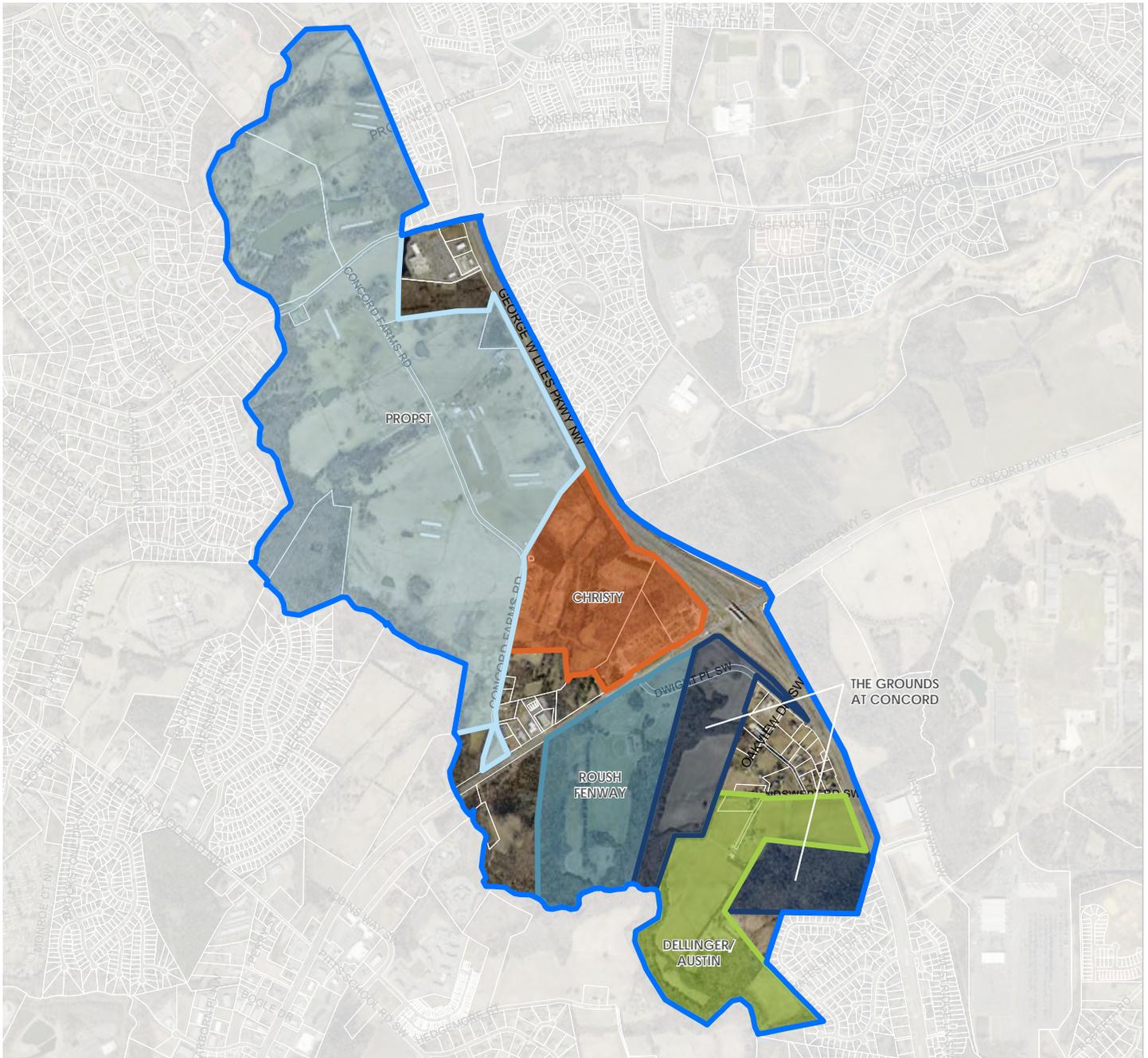
Suggested in the 2005 Concord Parkway/Roberta Church Road Small Area Plan, the Willow Oaks Crossing Shopping Center is located southwest of the George W. Liles Parkway and Weddington Road intersection. This grocery anchored neighborhood retail center is anchored by Concord's first Publix and total's 82,167 +/- sf of commercial use including shopping, dining, and other amenities.

Christy's Nursery

Christy's Nursery is located east of the creek along Concord Parkway and occupies approximately

(THIS PAGE INTENTIONALLY LEFT BLANK)

MAP 2.2: EXISTING DEVELOPMENT MAP - 2021 AERIAL IMAGE



20 acres. As part of the Christy family holdings, there are an additional 75 acres to the north of the nursery, some of which is used for mobile homes. This family-owned operation has been in this location for many years and provides landscaping materials and services to residential and commercial customers throughout the Charlotte region. Much of the nursery's stock is grown by the Christy family in other locations in North Carolina. The northern side of the current nursery operation is a tributary of Coddle Creek. This tributary is also within the Coddle Creek floodplain and contains a floodway.

North Side of Concord Parkway

There are several other small properties on about 38 acres between Coddle Creek and Concord Farms Rd. Some of it is rural/agricultural in use, some is commercial like the Nature's Own Bakery Outlet, Morrison Motor Company, Caliber Collision, Tracie Wells (Psychic), and Affordable Corporate Suites, some is vacant and available for sale, and some is isolated single family residential.

South Side of Concord Parkway/The Grounds at Concord

On the south side of Concord Parkway are larger tracts owned by the Roush Corporation of about 101 acres and a portion of The Grounds at Concord site located on two disconnected tracts that combined equal almost 86 acres. Currently, these tracts are mostly vacant: some are cleared and some are wooded. The other large piece of Area A is agricultural land of about 108 acres near the end of Windswept Rd. There's also about 36 acres in this section that is currently developed as a mixture of low-density residential and light industrial uses.

2.2.2. EXISTING INFRASTRUCTURE

Water

The existing Water System in Study Area A is limited to distribution main water lines running along Weddington Road (12 inch), George W. Liles Parkway (24 inch), and Concord Parkway (16 inch). These distribution main water lines also extend into the areas of existing development within Study Area A including the Willow Oaks Crossing Neighborhood Center, commercial properties along Concord Parkway and the low-density residential and light industrial uses south of Concord Parkway.

Area A sits at the eventual boundary of two zones from two different water treatment plants. The City of Concord has invested in water infrastructure to supply proposed and future large water users to the Grounds at Concord. Development within this area should be coordinated with the City early in the development process to assist in modeling water needs and availability.

Sewer

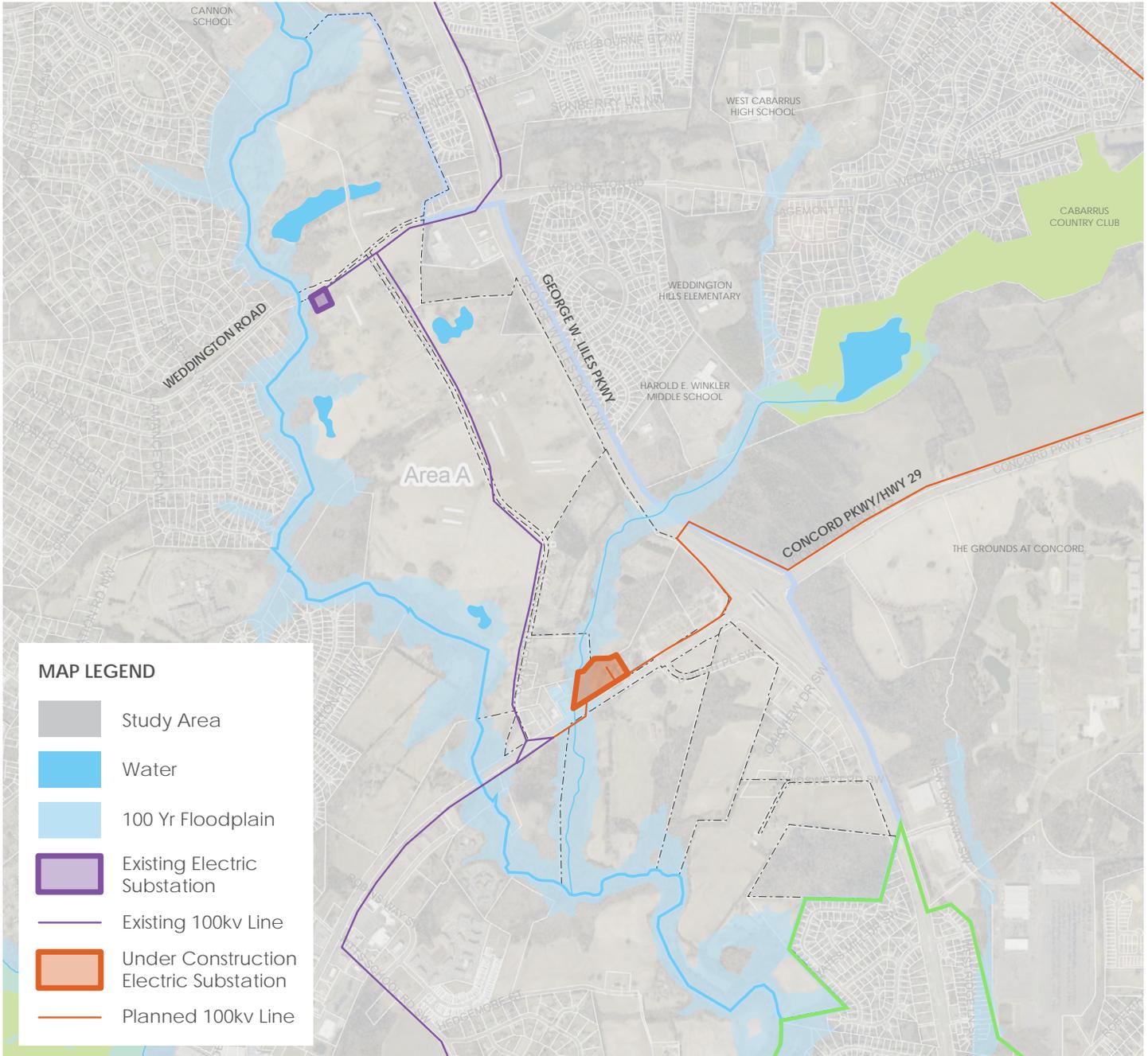
Sewer is development driven and existing sewer infrastructure in Area A is limited due to the lack of existing development in the area. Any proposed future development in this area would need additional Sewer added.

As development occurs developers will be required to work with the City to assess sewer needs and capacity. Sewer capacity discussions will involve The Water & Sewer Authority of Cabarrus County (WSACC) as well. The amount of sewer available for allocation and attributable by any designated calendar year is given by WSACC as determined through the interlocal agreement and Concord's City Council. The City's Sewer Allocation Policy can be viewed on the City's website at: <https://concordnc.gov/Departments/Engineering/Development-Standards>

Stormwater

With little hardscape areas within Study Area A the existing stormwater system is conveyed through ditches alongside the existing road network. As development occurs within this area, protection of the floodplain is critical. Unmanaged upstream urbanization of a floodplain results in a dramatic increase in the flood storage needs of the creeks as they traverse Concord. With an increase in the width of the floodplain comes a greater incidence of flooding and a reduction in overall amount of property available for development.

MAP 2.3: EXISTING ELECTRIC UTILITIES MAP



If this area is to thrive as a pedestrian-friendly, mixed-use center, it must have wide sidewalks, small lots, and buildings built close to the street and to each other. An important tool in managing stormwater quantity and water quality is the use of Low Impact Development (LID) Standards. LID is an ecologically friendly approach to site development and storm water management that aims to mitigate development impacts to land, water, and air. See Page 63 for more on LID.

Electrical Systems

An existing electrical substation is located on Weddington Road and occupies about one acre of land. The most significant electrical feature in Study Area A is the presence of very large 100kv electrical transmission poles along Concord Farms Road. The poles were installed by the City of Concord electric system. In addition to the high steel poles, there are also a number of wood poles that serve as local "drops" for the homes and buildings along the road. They also carry other aerial utility services such as telephone and cable.

A new electrical substation is currently under construction near the intersection of Concord Farms Road and Concord Pkwy/Hwy 29 and will help to serve developments at The Grounds at Concord as well as future developments within Study Area A. This project includes the installation of a new 100kv electrical transmission line as seen on Page 41.

Transportation

George W Liles Pkwy – The main North-South corridor along Study Area A is George W. Liles Pkwy. It is mostly a four-lane divided highway. Currently, there is one left-over near the Publix at Willow Oaks Crossing and a U-turn location about 750 feet south of the left over that allows for northbound traffic coming out of the shopping center. There is a grade-separated interchange at Concord Pkwy (Hwy 29), which may cause access challenges for new intersections onto George W Liles Pkwy north of Concord Parkway. There will be additional discussion of George W Liles Pkwy in sections regarding Study Area B. The NCDOT 2020 average daily traffic map indicates 22,500 vehicles per day (vpd) on George W. Liles Parkway north of Weddington Road. No recorded vpd are available for the Parkway south of Weddington Road.

Concord Farms Rd. – Concord Farms Rd. is currently a two-lane road that traverses north-south through the site connecting US 29 and Weddington Road. This road is primarily intended to provide access to adjacent agriculture activity on the east and west sides of the road. The road is approximately 20 feet in width with two, 9-foot travel lanes. The average daily traffic volumes were recorded as 5,700 vpd.

Weddington Road – Weddington Road is a two-lane roadway classified as a minor thoroughfare that collects traffic from adjoining neighborhoods. The roadway has ultimate connections to Bruton Smith Boulevard to the west. The average daily traffic volumes were recorded as 10,000 vpd west of George W. Liles Parkway. East of George W. Liles Parkway the roadway has a recorded volume of 13,500 vpd.

Concord Pkwy – Concord Pkwy (US Hwy 29) in the vicinity is a four-lane divided roadway with a posted speed limit of 55 mph. The roadway generally parallels I-85 within the immediate area and provides connections to the south at I-485 in Mecklenburg County and connections to the north to downtown Concord with ultimate connections north of I-85 to Kannapolis and Rowan County. The NCDOT average daily traffic count map indicates a recorded volume of 24,500 vpd on Concord Parkway in the vicinity of Study Area A.

Public Transit

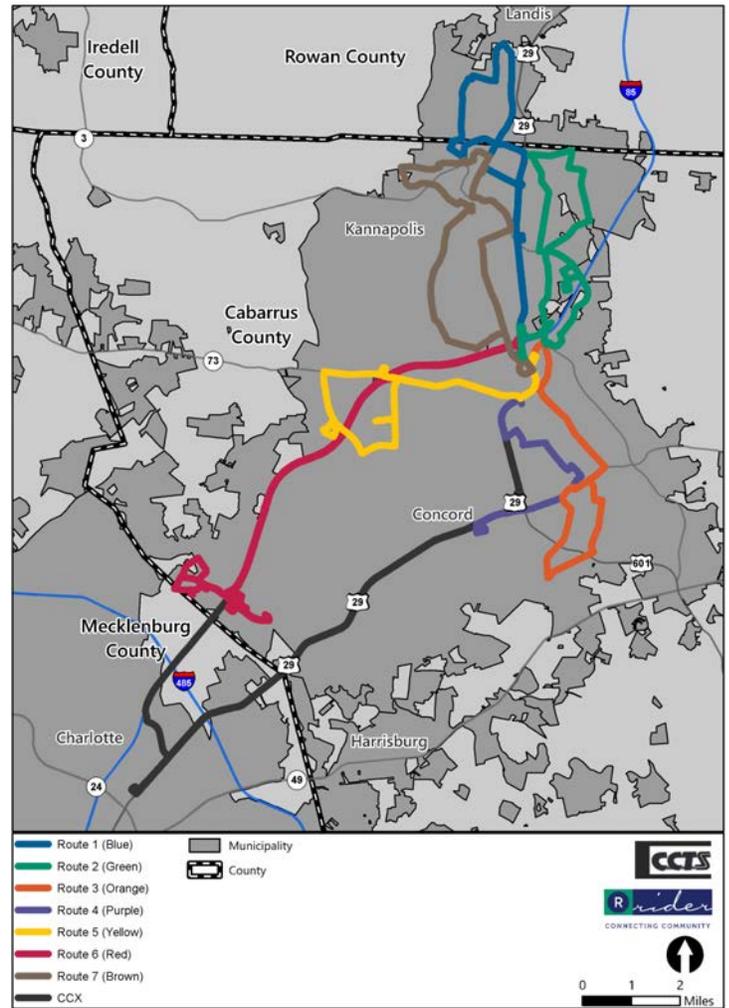
Concord Charlotte Express (CCX) – The one existing Fixed Bus Route that passes through Area A of this plan is the Concord Charlotte Express (CCX) Route, a regional express route which connects passengers traveling from Cabarrus County to the Charlotte metropolitan area. This route travels along Concord Parkway and is Rider Transit’s longest route at 28.91 miles. It is currently Rider’s only regional route, connecting to the CATS Lync Blue Line light-rail system at the JW Clay Station. According to the Cabarrus County Long Range Public Transit Master Plan (LRPT), the most popular stop on the CCX is the JW Clay Light Rail Station as would be expected, followed by The Village/ Big Lots stop, where a passenger park & ride lot is located.

This route is providing an important new connection between Cabarrus County and Charlotte seven days a week. In the short time frame that the route has operated, it has proved to be very popular. One limitation of the CCX route is that it does not run late. For passengers making additional route connections coming back from Charlotte, passengers must be on the 6:56pm departure from the JW Clay stop.

Route 5 (Yellow) – The next nearest Fixed Bus Route to Area A is Route 5 (Yellow) which has two stops along Poplar Tent Road near George W. Liles Pkwy and International Drive NW. The greatest boardings are at Rowan Cabarrus Community College, followed distantly by the Country Club Drive and Starbucks stop at Carolina Mall. Other important stops are at Amazon and Target in Afton Ridge, and multiple apartment complexes found along the route.

Opportunities for Route 5, according to the LRPT, include partnering with businesses to implement a ride share or van pool program. Ride share or van pool programs enable

MAP 2.4: CURRENT FIXED ROUTE SYSTEM



Rider Transit Fixed Bus Route Stop at the Rider Transit Center.

employees to share their commute to and from their place of work. This type of transit service may be better suited to the existing development pattern on this route than Fixed Route transit.

Parks & Recreation

Dorton Park – Coddle Creek, a meandering stream and floodplain along the western boundary of the study area has banks as wide as 12-16 feet in places. Further upstream, it has been incorporated into Dorton Park, a 24 acre park with tennis courts, playground equipment, soccer fields, Frisbee Golf, a 1.1 mile gravel walking trail, picnic shelters, restrooms and a concession stand. Dorton Park is located in Afton Village, a traditional neighborhood between Poplar Tent Road and George W. Liles Parkway.

The 100 year floodplain surrounding Coddle Creek is quite significant even though its headwaters are impounded as Lake Howell. Coddle Creek travels southeast past Frank Liske Park and eventually ties into the Rocky River.

Fire Department

This area of Concord is currently served by Fire Stations No. 5 and No. 7.

Fire Station No. 5 – Dedicated in 1991, Concord Fire Station No. 5 is located at 170 Pitts School Road. Originally operated by the Pitts Volunteer Fire Department, the structure was renovated and expanded in 1991 as a result of an agreement between the Pitts VFD Board of Directors and the City of Concord for the Pitts Fire District to be protected by the Concord Department of Fire & Life Safety, and that all equipment, vehicles, and structures owned by the Pitts Fire District be given to the City of Concord. Thus, since 1991, Concord Engine Company 5 has been responsible for this area. Engine Company 5, and a reserve engine is quartered in this station.

Fire Station No. 5's first due area includes the Charlotte Motor Speedway, a mixture of residential, multi-family, industrial, commercial occupancies as well as the Hwy. 29 and Speedway Boulevard corridors.

Fire Station No. 7 – Dedicated in 2001, Concord Fire Station No. 7 is located at 250 International Drive NW. In addition, this facility has a Community/Training Room at this location and it is available to the public. Engine Company 7, Ladder Company 7, and Hazmat 7 are quartered in this station.

Fire Station No.7's first due area includes a mixture of residential, multi-family, industrial, commercial occupancies as well as I-85 and the Hwy. 73 corridor.

Environmental

Map 2.4 was created using the USDA Soil Data Explorer tool and applies to dwellings with or without a basement and small commercial structures. The soil map includes the map for the defined area of interest, a list of soil map units on the map and extent of each map unit, and cartographic symbols displayed on the map. Below is a description of the various soil types found in this area.

Soil Types

MeB: Mecklenburg loam, 2 to 8 percent slopes (624.9 acres, 52.0%)

IdA: Iredell loam, 0 to 2 percent slopes (334.2 acres, 27.8%)

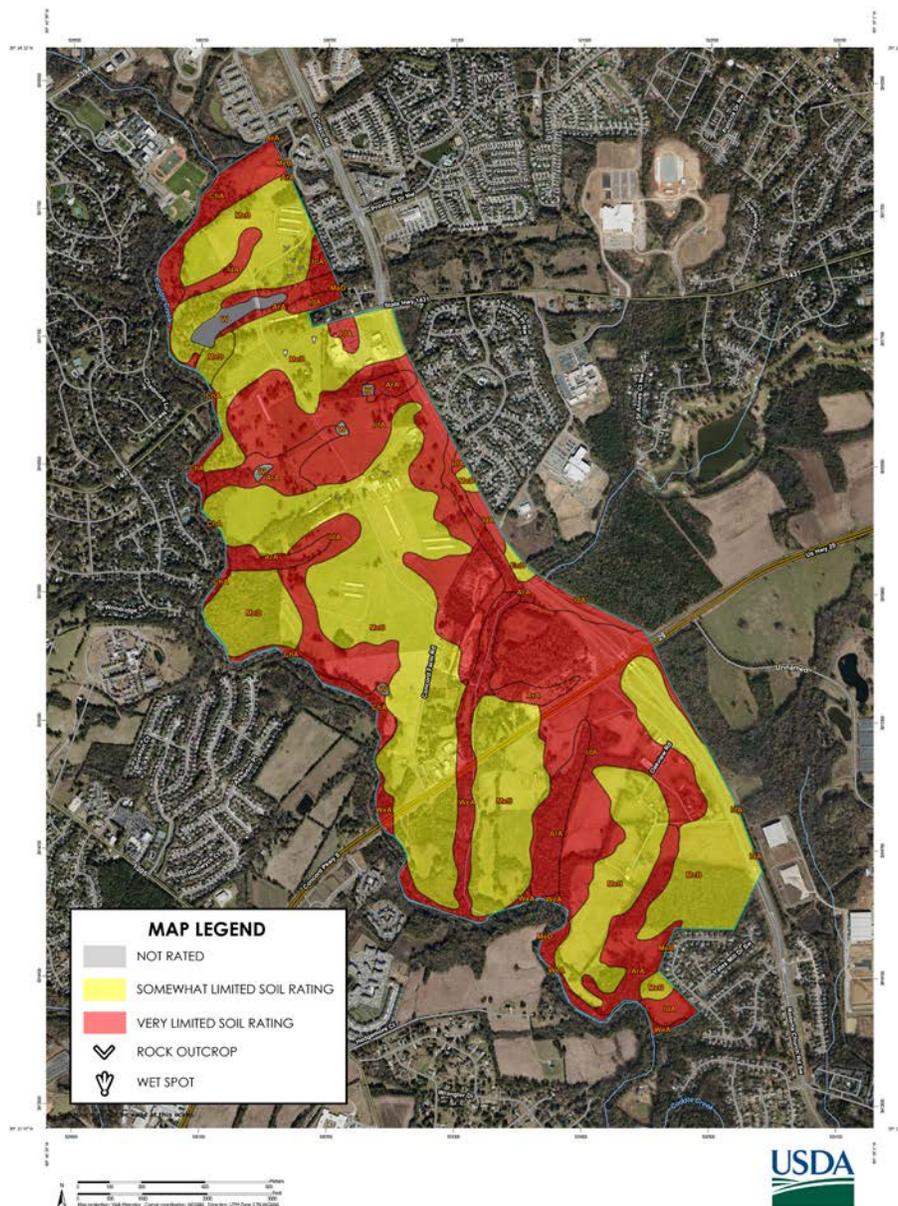
ArA: Armenia loam, 0 to 2 percent slopes, rarely flooded (129.4 acres, 10.8%)

Remaining 9.4%:

- WeA: Wehadkee loam, 0 to 2 percent slopes, frequently flooded (48.7 acres, 4.1%)
- ChA: Chewacla sandy loam, 0 to 2 percent sloped, frequently flooded (44.4 acres, 3.7%)
- W: Water (11.4 acres, 1.0%)
- MeD: Mecklenburg loam, 8 to 15 percent slopes (5.9 acres, 0.5%)
- EnB: Enon sandy loam, 2 to 8 percent slopes (2.9 acres, 0.2%)

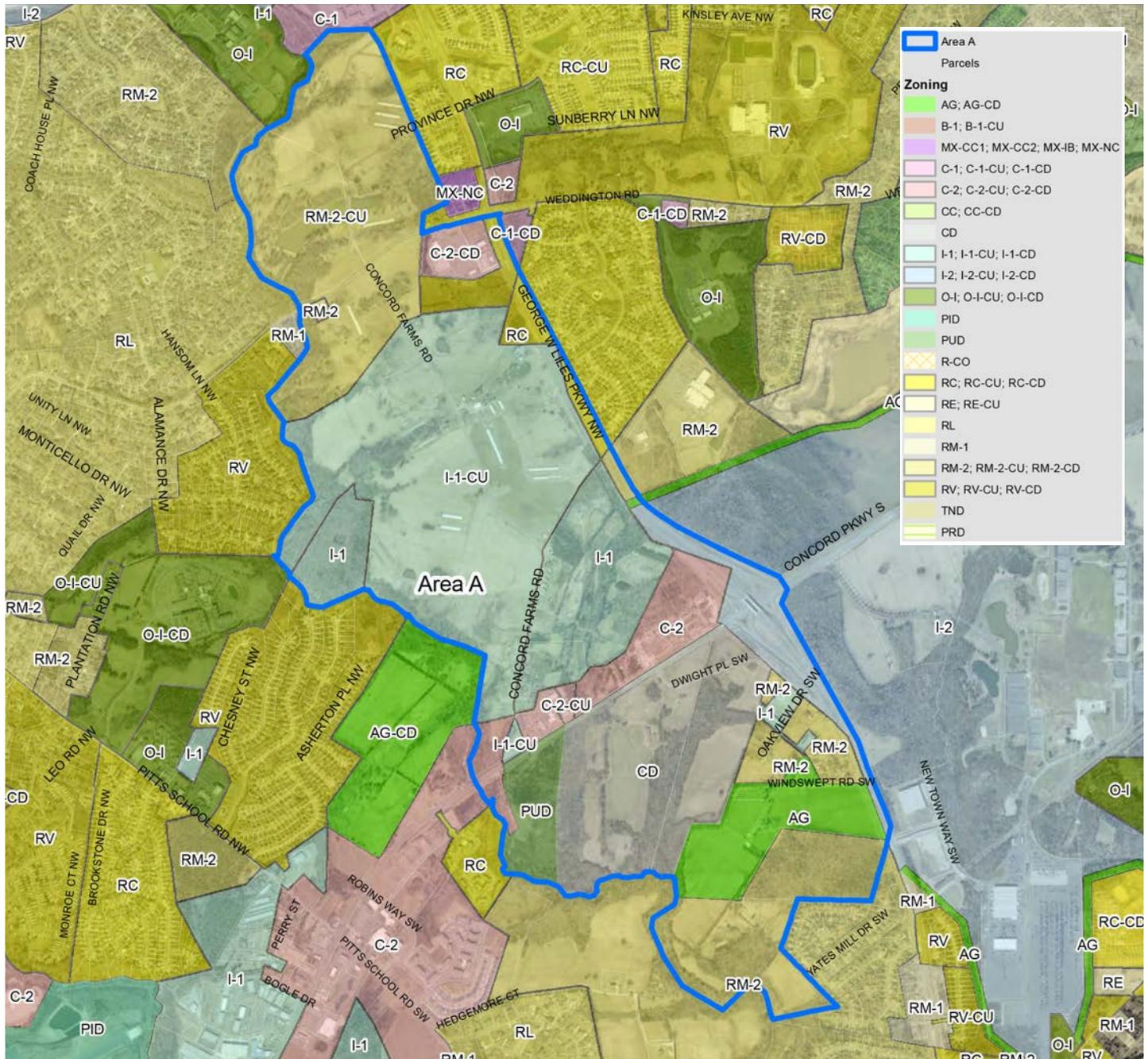
The soil types present in this area are consistent with of those in the surrounding properties. It should be noted that this is an assessment of the existing conditions and a “somewhat limited” or “very limited” soil rating is not an indication that development cannot occur in an area but that the soil may require amendments and modification.

MAP 2.5: USDA SOIL DATA EXPLORER MAP - DWELLINGS & SMALL COMMERCIAL STRUCTURES

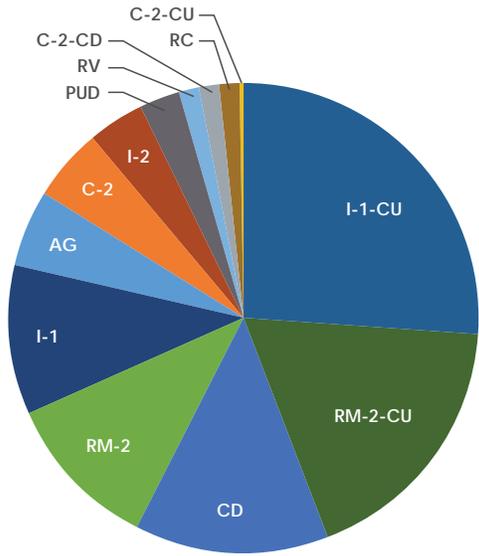


Source: <https://websoilsurvey.sc.egov.usda.gov/App/WebSoilSurvey.aspx>

MAP 2.6: AREA A - EXISTING ZONING MAP



STUDY AREA A | EXISTING CONDITIONS



AREA A PERCENT LAND AREA BY ZONING

ZONING	ACRES	%
AG	62.1	5.1%
C-2	61.6	5.1%
C-2-CD	16.8	1.4%
C-2-CU	2.8	0.2%
CD	162.1	13.3%
I-1	124.7	10.3%
I-1-CU	319.6	26.3%
I-2	48.9	4.0%
PUD	31.3	2.6%
RC	15.8	1.3%
RM-2	132.9	11.0%
RM-2-CU	217.2	17.9%
RV	18.4	1.5%
TOTAL	1,214.1	100%

2.2.4. EXISTING ZONING

Zoning establishes a list of uses that are permissible on a particular parcel, which is driven by the Concord Development Ordinance. While particular uses are subject to change over time, the general purpose of each zoning district is relatively stable.

Over 100 Acre Zoning Districts

I-1 – Light Industrial: Mix of light manufacturing uses, office park and limited retail and services in business park setting. Should have direct access to thoroughfare. 442.1 acres, Area A.

RM-2 – Residential Medium Density: Medium density Single Family; 4 dwelling units per acre (dua) max., where adequate public facilities exist. 350.1 acres, Area A.

CD – Campus Development: Mixture of employment and/or institutional uses; may include light manufacturing, office, warehousing, institutional and limited retail in campus setting with architectural design standards. 162.1 acres, Area A.

Includes supplemental design standards:

- No retail establishment greater than 70,000 square feet (sf) gross floor area (GFA)
- No more than 30% of property may be used for retail or commercial services
- Minimum of 20 acres
- Open space must comprise 20% of gross project area
- All buildings or clusters shall be connected with sidewalks, paths, bike ways
- Architectural Style and Appearance Requirements apply

Under 100 Acre Zoning Districts

AG – Agricultural District: Agricultural preservation areas. 62.1 acres, Area A.

C-2 – General Commercial: General commercial activities; located on or near major thoroughfares. 81.2 acres, Area A.

PUD – Planned Unit Development: Planned Unit Development districts (PUD) provide for the orderly development of land with a mix of land uses and intensities. PUD zoning is intended to permit innovation and flexibility in the design, construction and processing of mixed-use developments in exchange for the developer providing enhanced design elements. 31.3 acres, Area A.

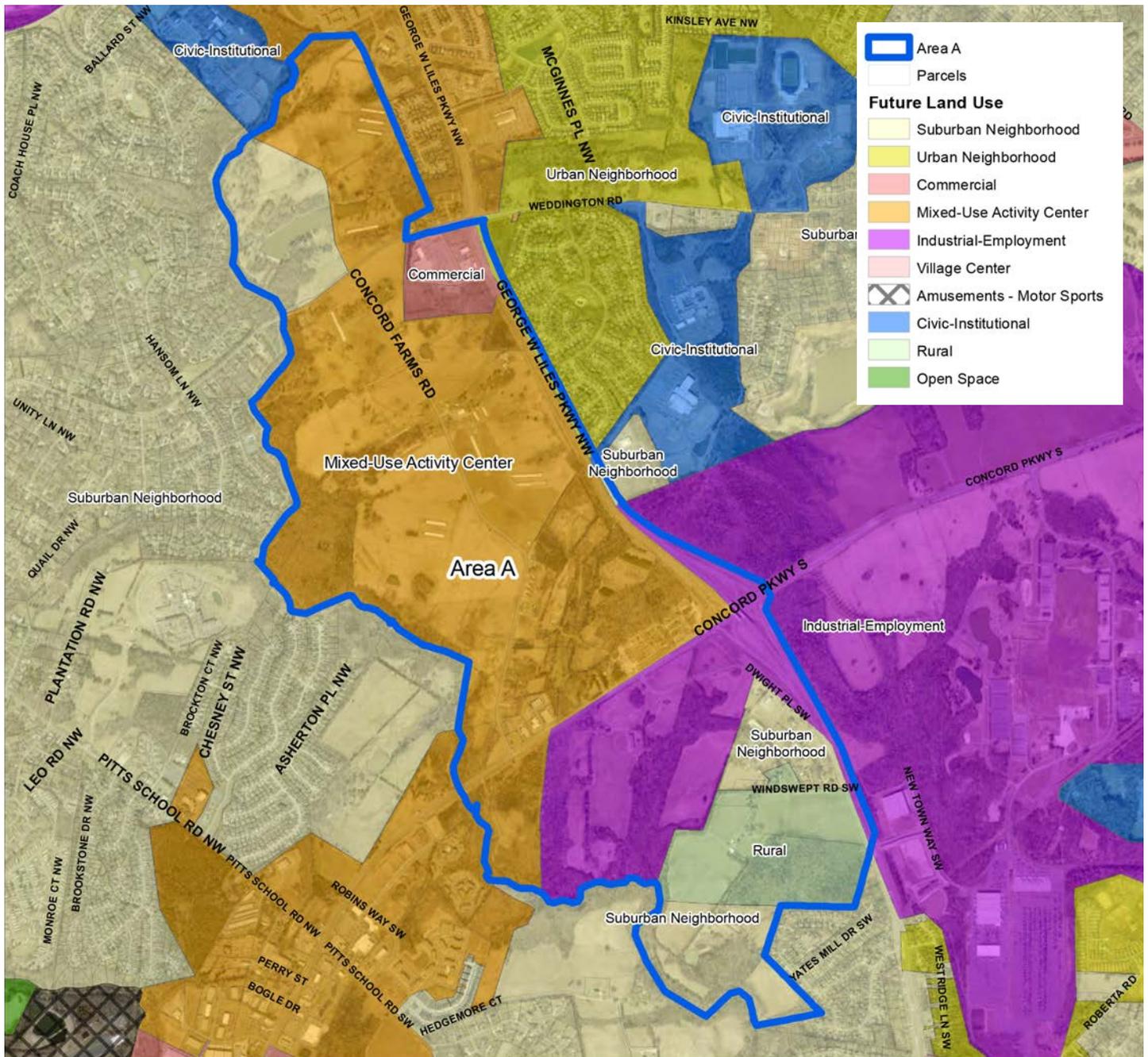
RV – Residential Village District: Detached and attached Single Family with max. of 8 dwelling units per acre. 18.4 acres, Area A.

RC – Residential Compact: High density residential including Single Family attached and multi-family, up to 15 dwelling units per acre. 15.8 acres, Area A.

-CU indicates that a site has a Conditional Use Zoning.

-CD indicates that the site is a Conditional District.

MAP 2.7: AREA A - FUTURE LAND USE MAP

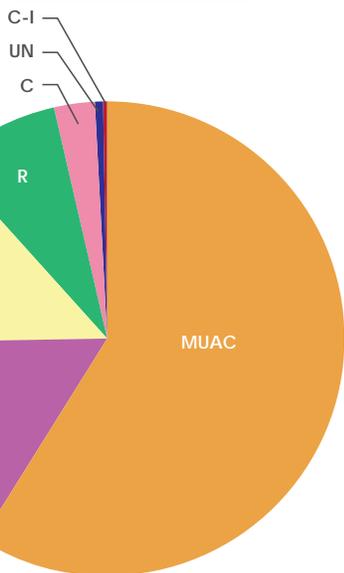


Area A

- Parcels

Future Land Use

- Suburban Neighborhood
- Urban Neighborhood
- Commercial
- Mixed-Use Activity Center
- Industrial-Employment
- Village Center
- Amusements - Motor Sports
- Civic-Institutional
- Rural
- Open Space



AREA A FUTURE LAND USE CATEGORIES

CATEGORY	ACRES	%
MIXED-USE ACTIVITY CENTER	715.4	58.9%
INDUSTRIAL-EMPLOYMENT	192.2	15.8%
SUBURBAN NEIGHBORHOOD	167.2	13.8%
RURAL	97.2	8.0%
COMMERCIAL	32.9	2.7%
URBAN NEIGHBORHOOD	8.9	0.7%
CIVIC-INSTITUTIONAL	0.3	0.0%
TOTAL	1,214.2	100%

2.2.5. 2030 LAND USE PLAN

Central Concord District

Much of the Study Area is included in the “Central Concord Character Area” from the City of Concord 2030 Land Use Plan (see Map 2.X):

The Central Concord Character Area extends southwest along Concord Parkway (US 29) from Wolf Meadow Branch (west of the Wal-Mart Super Center) past the corridor’s intersection with Pitts School Road. There are two planned Mixed-Use activity Centers along Concord Parkway – one at George W. Liles Parkway and one at Pitts School Road. While largely undeveloped today, the Concord Parkway Corridor and the adjacent areas that represent Central Concord are planned to accommodate significant amounts of residential, commercial, and industrial development over the next ten years. As this growth occurs, the City will face the following challenges:

- Maintaining Concord Parkway as an attractive corridor that serve as a primary gateway to Downtown Concord.
- Serving a mix of travel modes along Concord Parkway, including motorists, transit (both bus and the long-term potential for light rail/commuter rail), bicyclists and pedestrians (particularly within the activity centers).
- Providing multi-modal connections between uses within the activity centers.
- Ensuring compatible transitions between industrial commercial, and residential land uses.

While future rail service from Concord to Charlotte is not a given, the City has a strong interest in preserving the opportunity for future service along the Concord Parkway corridor, which provides the most direct route from Downtown Concord to the existing Blue Line lynx service at UNC Charlotte. The northeastern portion of this character area has abundant open land to accommodate future rail right-of-way on either side of the Concord Parkway corridor. The southwestern portion of the character area has less vacant land available within existing right of way, but retains opportunities on private property to provide parallel right-of-way. The city will need to identify and protect potential right-of-way for a preferred alignment to ensure that future rail services remains viable in this corridor.

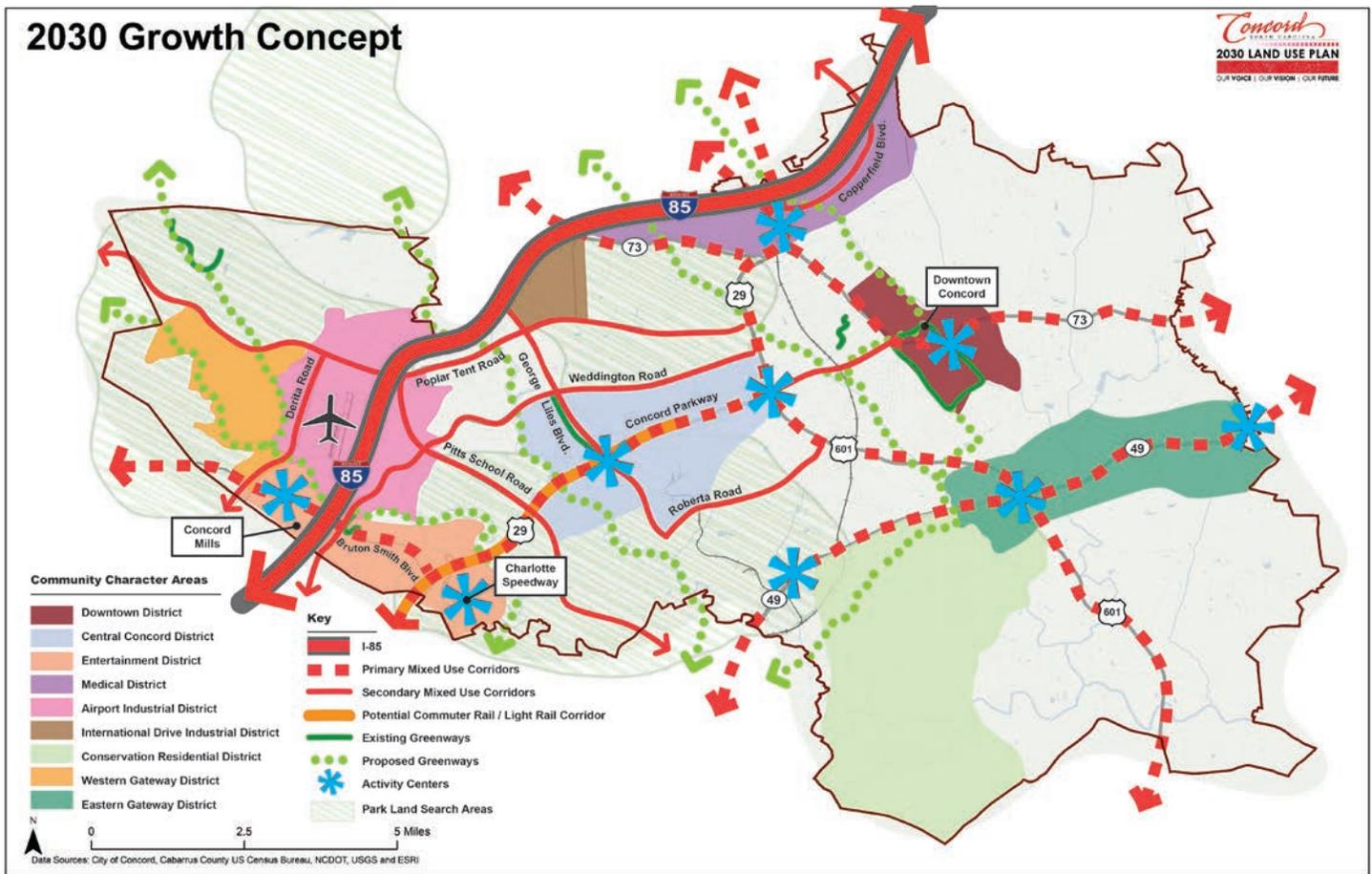
Future Land Use Categories

The entire corridor of study encompasses almost every future land use category in the 2030 Land use Plan.

Mixed-Use Activity Center – In the northern area of the small area plan, the majority of land is designated as Mixed Use Activity Center: 715.4 acres, or 58.9% of the land area of Area A.

The intent of the Mixed-Use Activity Center Future Land Use category is to identify areas that serve broader economic, entertainment, and community activities as compared to mixed-use neighborhoods. Uses and buildings are located on small blocks with streets designed to encourage pedestrian activities. Buildings in the core of the mixed-use center may stand three or more stories. Residential units or office space may be found above storefronts. Parking is satisfied by using on-street parking, structured parking, and shared rear-lot parking strategies. Structured parking should be encouraged to be incorporated into the principal structure.

MAP 2.8: 2030 LAND USE PLAN GROWTH CONCEPT MAP



A large-scale mixed-use activity center may be surrounded by one or more neighborhoods that encourage active living, with a comprehensive and interconnected network of walkable streets. Areas designated as Mixed-Use Activity Centers Future Land Use category are large geographic areas located at key intersections of major transportation corridors where transit facilities are currently planned or envisioned in the future. Mixed-Use Activity-Centers may not necessarily be constructed as a single development at one time. The very nature of these areas allows them to be approved in phases and to evolve over time into an area made up of a mix of land uses, usually centered on some type of public transit facility, which affords people the opportunity to live, work, shop and, in some cases, have immediate access to recreation facilities in one general area. It is also the goal of these districts to reduce vehicle trips by making them single destination points for multiple consumer needs.

The intent of the Mixed-Use Activity Center designation is to encourage the development of a mixture of complementary uses that will function as an integrated center allowing for pedestrian connections between developments and uses. Each of the eleven proposed Mixed-Use Activity Center locations identified on the Land use Plan have different characteristics and will develop uniquely. The size of these areas is relatively large, with the intent that mixed use development should occur within a one-half mile radius of a transit stop (an estimated ten-minute walking distance). Due

to the fact that each area designated as a MUAC is unique, applying one development solution to each is not practical. The key is to coordinate development of each parcel within the context of the larger District. Coordinating connectivity and planning for future uses will be critical in this process.

Industrial-Employment – The second largest land area in Area A of the Small Area Plan is Industrial/Employment: 192.2 acres, or 15.8% of Area A.

The intent of the Industrial/Employment (IE) Future Land use category is to identify those areas that have either already developed as industrial or are suited for additional industrial development due to the presence of infrastructure and access to transportation routes, such as major highways and railroads. These industrial areas should be preserved for employment uses to generate jobs for the community.

Industrial uses have already developed in several areas throughout the community, most of which are light industrial in function and impact. The 2030 Plan identifies the need to protect industrial lands, and to encourage additional growth in industrial/employment uses in designated areas, particularly around Concord Regional Airport, and along I-85 between Pitts school Road and Rocky River. Additionally, some light industrial/employment uses are encouraged to locate in Mixed-Use Districts, depending upon their intensity, as identified earlier in this section.

Suburban Neighborhood – The third largest land area in Area A is Suburban Neighborhood: 167.2 acres, or 13.8% of Area A.

The Suburban Neighborhood Future Land use category includes single-family areas that are formed as subdivisions or communities, with a relatively uniform housing type and density throughout. They may support a variety of single-family detached residential types, from low-density single-family homes to denser formats of smaller single-family homes. In areas designated Suburban Neighborhood, homes are typically buffered from surrounding development by transitional uses or landscaped areas. Single-family attached dwellings are an option for infill development within the suburban neighborhood future land use category. These neighborhoods are often found in close proximity to suburban commercial, office, and industrial centers, and help provide the consumers and employment base needed to support these centers. Lots at the intersections of collector and arterial streets within or at the edges of suburban neighborhoods may support small-scale, neighborhood serving, pedestrian-oriented commercial or service uses such as coffee shops, cafes, beauty salons and light retail. Pedestrian oriented uses exclude uses with drive-in, drive-through or automobile related services.

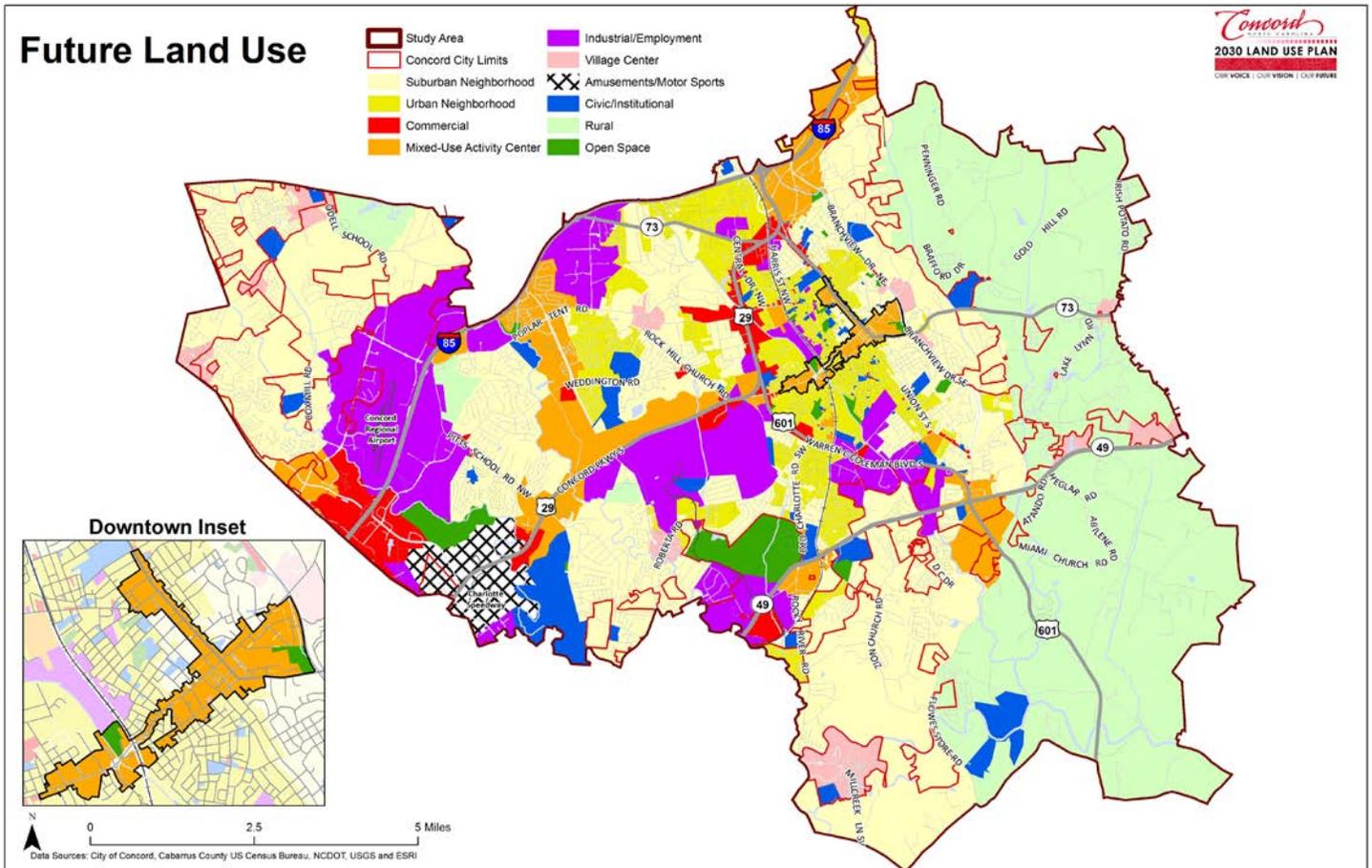
Rural – The Rural area consists of a small area of about 97.2 acres, or 8% of Area A. This area was maintained as a buffer between an established neighborhood and an area that is largely undeveloped and currently rural in character with AG or RM-2 zoning. The purpose of this designation was to act as a placeholder until more information about future development became available. As a result of this small area plan, the future land use designation of the rural piece and those sections approaching the southwest quadrant of the intersection of George Liles Pkwy and Concord Pkwy are being reevaluated.

The intent of the Rural Future Land Use category is to identify a variety of land use types that are representative of working agricultural uses as well as a variety of residential types including farmhouses, to large acreage rural family subdivisions, to ecologically-minded “conservation subdivisions” which are designed to preserve open landscape, and traditional buildings, often with a mixture of residential and agricultural/rural-supported commercial uses.

Commercial – The Commercial area is limited to 32.9 acres, or 2.7% of Area A. The Commercial future Land Use category includes a mix of commercial land use types. While these areas continue to support additional commercial development and redevelopment, much of the new commercial development should be concentrated within the Mixed-Use activity Centers and Village Centers. Strip commercial development along major corridors is discouraged in the 2030 Plan. Instead commercial development integrated into Mixed-Use activity Center at key intersections is desired. Areas designated as Commercial are intended to represent those that include a variety of commercial uses at different intensities, including large scale malls, lifestyle centers, and community shopping centers.

Urban Neighborhood – There are trace amounts, 8.9 acres, or .7% of Area A that are designated as Urban Neighborhood.

MAP 2.9: FUTURE LAND USE MAP



2.3. FUTURE FOCUS

2.3.1. LAND USE & BUILT FORM

Lifestyle Focus

In Area A, 85% of the 170 input survey respondents indicated that they wanted to see Recreation, 73% indicated Dining, and 65% Entertainment. The market study identified 105,000 square feet of current demand for retail/entertainment within the study area. Staff expects the demand for commercial and retail uses to increase as properties develop with additional employment and residential uses. Therefore, developers should leave some capacity to accommodate this future demand.

Similar to the 2005 Roberta Church Road/Concord Pkwy Plan, this plan envisions the concentration of a mixed-use village center centrally located within the Study Area with access from George W. Liles Pkwy and Concord Farms Road about halfway between Weddington Road and Concord Pkwy.

Walkability is a key component of a vibrant lifestyle focused village center. This plan's vision statement and the associated focus on a connected community support a multi-modal linkage along the entire corridor via robust streetscapes and significant greenway investment, especially considering that the intersection of the Coddle Creek Greenway and Old Wagon Road Greenway are anticipated to fall within the Study Area.

Within the lifestyle center, buildings should rise to a maximum of 4 to 5 stories with either a horizontally or vertically integrated mixture of uses. Taller buildings within the lifestyle center should be clustered closer to the main entry at George W. Liles Pkwy with decreasing intensity as the development reaches Coddle Creek. Buildings at the entry should have retail or office on the ground floor.

Developers should consider incorporating an open space/plaza space between the buildings in the design of the lifestyle center, as demonstrated in the Mooresville Town Square and Blakeney Shopping Center precedent imagery.



Blakeney Shopping Center - Charlotte, North Carolina



Mooresville Town Square Rendering - Mooresville, NC

Additional design guidelines and recommendations for the lifestyle center are included in the Development Guidelines in the appendices.

Workplace Focus

Workplace-focused land uses should cluster around Concord Parkway to the north and the south along the corridor. The market study presented a demand for 960,000 square feet of industrial space and 112,000 square feet of office/institutional space. Some economists expect a resurgent demand for suburban office space in the post-COVID-19, therefore there may be an opportunity for this usage in the near term.

The Roush and Bootsmead properties on the south side of Hwy 29 provide the principal opportunity for industrial projects. The Christy property north of Hwy 29 is well suited for a corporate/office presence. Industrial projects throughout these areas should have enhanced design standards when fronting Hwy 29 in order to maintain a quality gateway along Hwy 29.



Ballantyne | Charlotte, NC



Peak City Business Park | Apex, NC



Griffith St. Traffic Circle Welcome Sign | Davidson, NC



Griffith St. Traffic Circle at I-77 Exit 30 | Davidson, NC



Music Factory | Charlotte, NC



Top Golf | Charlotte, NC



City Springs | Sandy Spring, GA

Single-family residential uses are not supported adjacent to the intersection of Hwy 29 and George W. Liles Pkwy or along Hwy 29 within the Study Area. However, a hotel, additional commercial/retail uses or vertical mixed-use would incorporate well into the workplace focus area to support the employment base.

Entertainment uses would complement with the workplace land uses in the form of a facility like a Top Golf, a Performing Arts Center, or a Music Factory.

Policy and decision makers should maintain connectivity infrastructure standards and aesthetic standards from the Campus District zoning district for buildings fronting George W Liles Pkwy or Hwy 29. Development Guidelines for Study Area A are available in the appendices for more information.

Residential

The market study projects demand for 350-726 units of residential per year. Staff anticipates residential development will be concentrated north of Weddington Rd., between the lifestyle center

and workplace areas on the Propst property. Residential development on the Propst property should be concentrated and phased to begin west of Concord Farms Rd., reserving the area to the east of Concord Farms Rd. for later phases or other potential uses. The view shed along George W Liles Pkwy should be maintained as long as possible.

Developers should integrate residential housing types, which are reflected in the Development Guidelines in the appendices and demonstrated in the Baxter Village Development in Fort Mill, SC.

Staff anticipates residential development will occur on the Cline/Austin/Dellinger properties south of the Roush and Bootsmead property as well as on the Bootsmead property south of the Cline/Austin/Dellinger properties. Developers should adequately transition and provide buffering between residential development and the anticipated workplace development that bounds the properties.

Open Space

Open Space is a central part of this plan and was a recurring theme within the public input survey comments. Some open space must be preserved within the substantial flood plain along Coddle Creek. Existing zoning calls for activating the floodplain with greenway and is therefore expected as part of the development of Area A. Existing zoning on the Propst property calls for enhanced buffering beyond Coddle Creek for the I-1-CU portion of the property up to 125’ from the top of the bank of Coddle Creek. The City’s Parks & Recreation Greenway Plan proposes to route the Old Wagon Road Greenway along the current alignment of Concord Farms Rd. towards George W. Liles Pkwy. The City’s Parks & Recreation Department may pursue some active recreation opportunities



Baxter Village | Fort Mill, SC



Baxter Village | Fort Mill, SC

WORKPLACE-FOCUSED

 **Office/Institutional**
112,000 SF

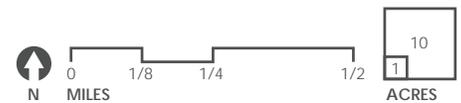
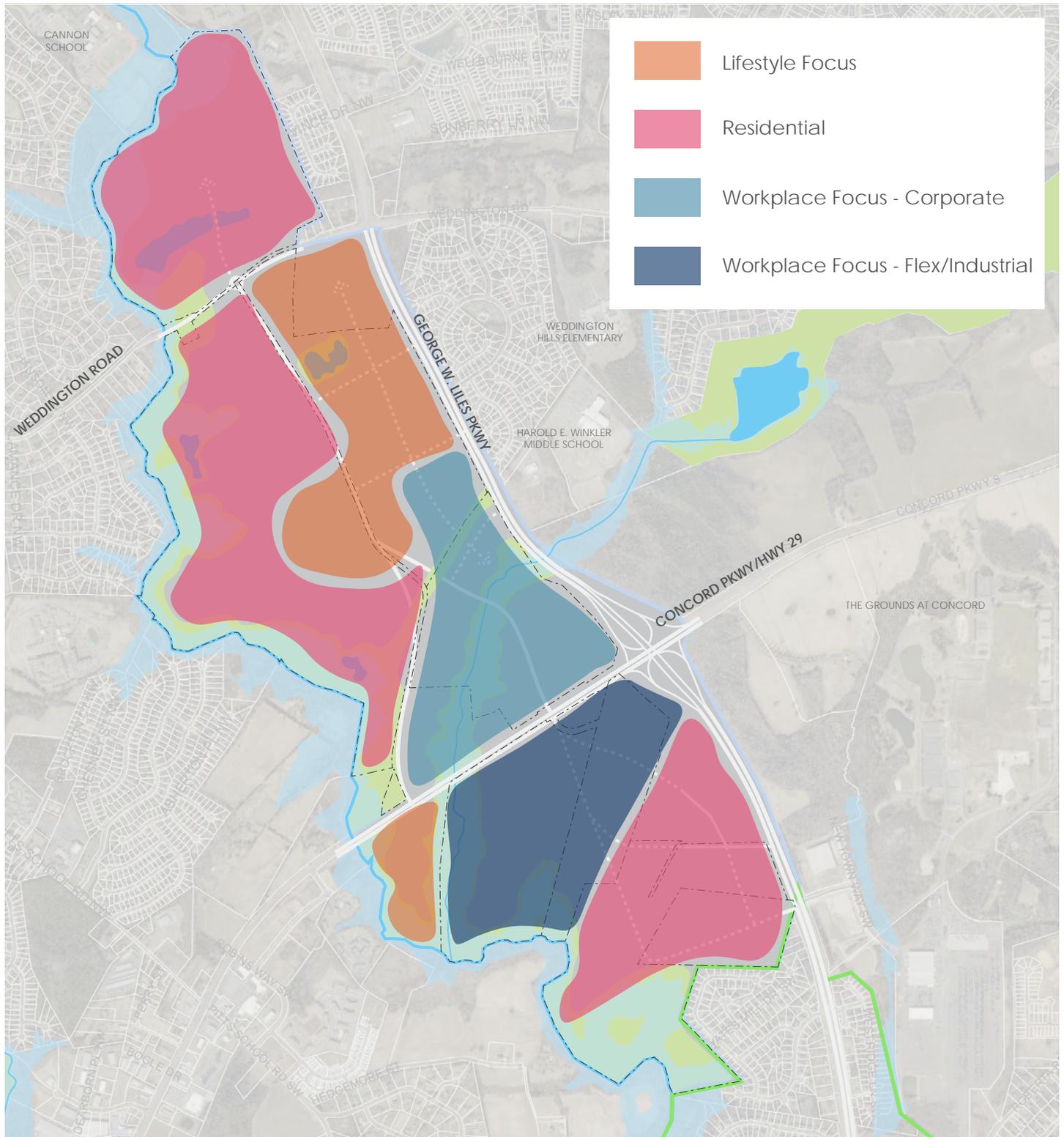
 **Industrial/Flex**
960,000 SF

LIFESTYLE-FOCUSED

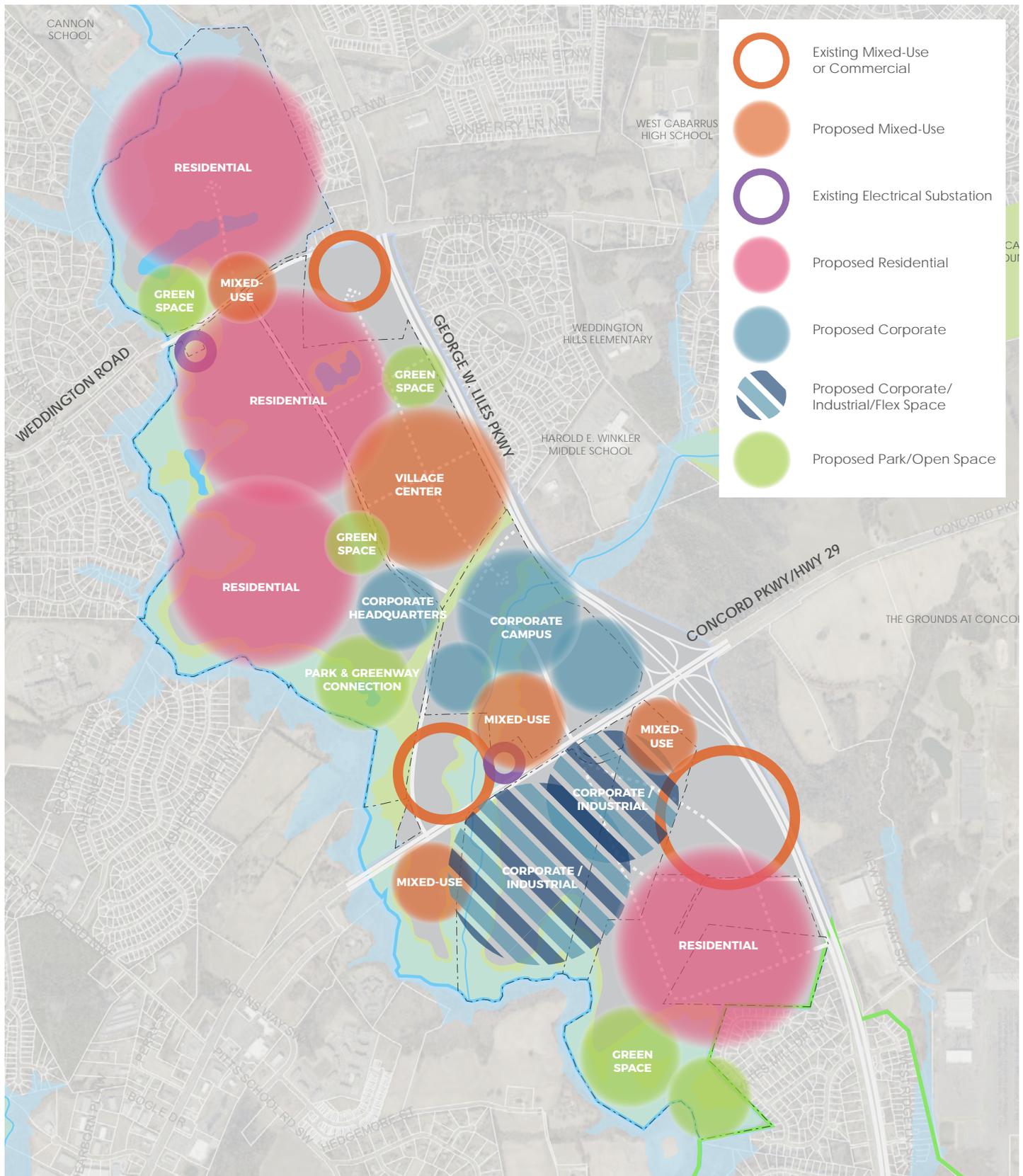
 **Retail/Entertainment**
105,000 SF

 **Housing**
350-726 units/year

MAP 2.10: AREA A - LAND USE FOCUS DIAGRAM



MAP 2.11: AREA A - LAND USE BUBBLE DIAGRAM



There are many ways in which this area, as a Multi-Use Activity Center w/ Industrial Employment, could develop. The bubble diagram above is one out of many possibilities. It should be taken as a suggestion and re-evaluated based on market demand and surrounding developmental impacts as development occurs within Study Area A.

in the Study Area and has expressed a need for field space. It would be beneficial to locate such a use near the intersection of the Great Wagon Rd. and Coddle Creek Greenway. In addition to public recreation facilities, developers should coordinate a private trail system that will provide links and accesses between residential neighborhoods, the public greenway system, the lifestyle center, and workplace areas. Open space may take the forms of plazas, fountains, and other semi-public amenities within the lifestyle and workplace areas.

Playable public art would add some place-defining elements and interactive activities along the new Concord Farms Rd. cross section or along the Coddle Creek Greenway. The City could play a substantial role in coordinating and partnering with developers to help with the creation and installation of public art.

Agricultural food production would benefit the area by improving the availability to healthy and locally grown food. Developers might achieve this by either reserving a portion of the land for farming operations or by allocating space for community gardens. Expecting that the properties will develop in phases, agricultural operations could be amortized gradually as development proceeds. Developers could pursue some locavore or agrihood development if agriculture is identified as a defining characteristic of the development.

Low Impact Development

Low impact development standards benefit the environment and are desired components in future development. As demonstrated in much of the precedent imagery in the other



Abernathy Park Playable Public Art- Sandy Springs, Georgia



Freedom Park - Charlotte, North Carolina



Abernathy Park - Sandy Springs, Georgia

sections, intensive uses are typically surrounded by a sea of parking. This is not the desired end-state for the study area. Developers should incorporate green infrastructure to the extent practicable, but at a minimum for 25% of the anticipated runoff from the development. Furthermore, developers should plant trees throughout parking lots to soften their appearance with no more than ten parking spaces in a row without trees.



Ferrington Village | Ferrington, NC



Olivette A Asheville, NC



Serenbe | Chattahoochee Hills, GA

GREEN INFRASTRUCTURE ELEMENTS

Bioswales



Bioswales are vegetated, mulched, or xeriscaped channels that provide treatment and retention as they move stormwater from one place to another. Vegetated swales slow, infiltrate, and filter stormwater flows. As linear features, they are particularly well suited to being placed along streets and parking lots.

Permeable Pavers



Permeable pavements infiltrate, treat, and/or store rainwater where it falls. They can be made of pervious concrete, porous asphalt, or permeable interlocking pavers. This practice could be particularly cost effective where land values are high and flooding or icing is a problem.

Green Streets



Green streets are created by integrating green infrastructure elements into their design to store, infiltrate, and evapotranspire stormwater. Permeable pavement, bioswales, planter boxes, and trees are among the elements that can be woven into street or alley design.

Land Conservation



The water quality and flooding impacts of urban stormwater also can be addressed by protecting open spaces and sensitive natural areas within and adjacent to a city while providing recreational opportunities for city residents. Natural areas that should be a focus of this effort include riparian areas, wetlands, and steep hillsides.

Source: <https://www.epa.gov/green-infrastructure/what-green-infrastructure>

(THIS PAGE INTENTIONALLY LEFT BLANK)

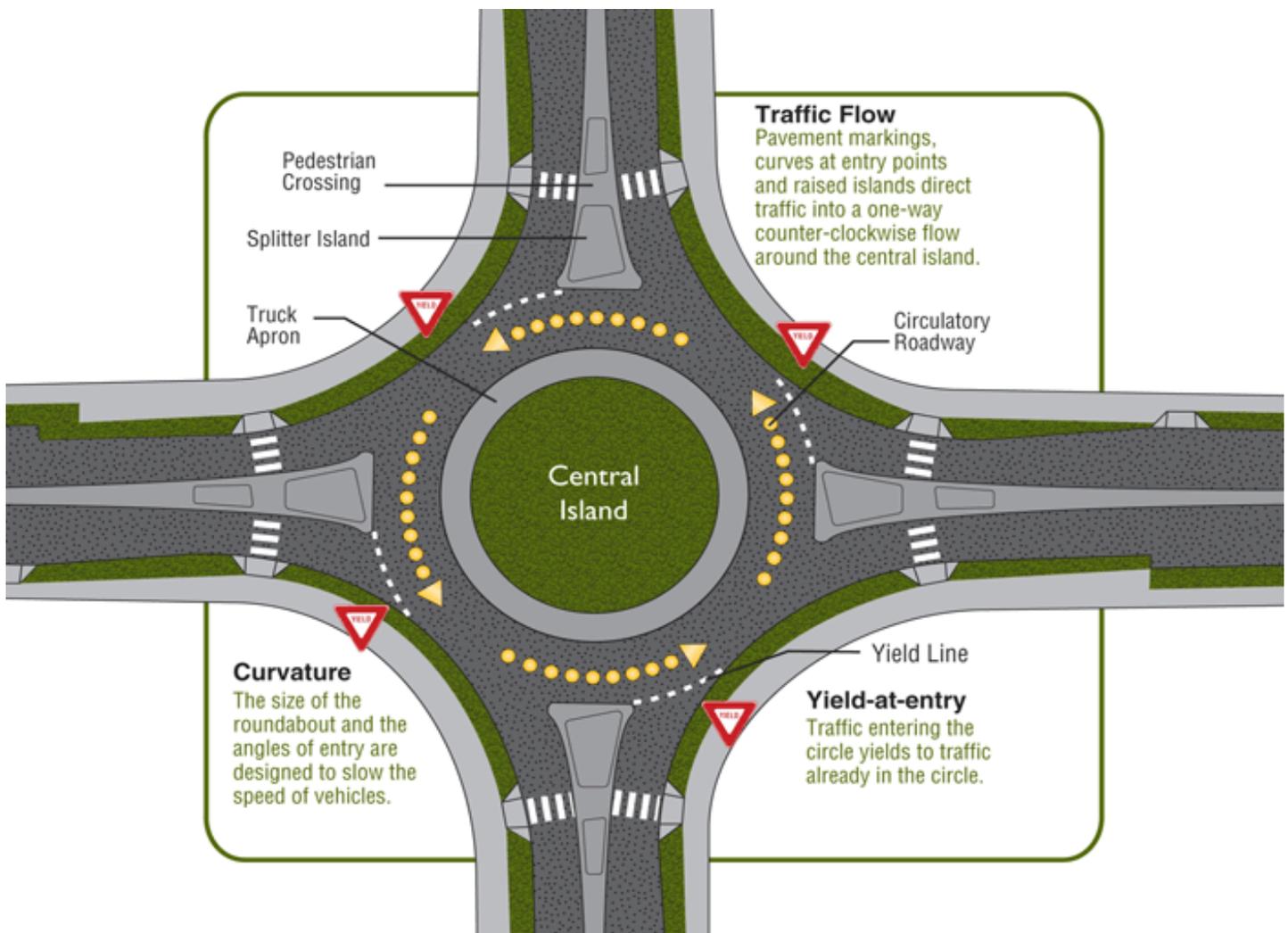
2.3.2. TRANSPORTATION

The following transportation recommendations are a result of information gathered through meetings with the Transportation Department, NCDOT, key property owners and comments gathered from public input.

Roundabout at Weddington Road

The existing T-intersection at Concord Farms Road and Weddington Road is currently a one way stop. Given the proposed development around this intersection and its proximity to the existing signaled intersection at George W. Liles Pkwy, this plan recommends a new roundabout to be constructed at this intersection.

The purpose of a roundabout is to eliminate left turns by requiring traffic to exit to the right of the circle. Roundabouts are installed to reduce vehicular speeds, improve safety at intersections through eliminating angle collisions, help traffic flow more efficiently and reduce operation costs when converting from signalized intersections, and help create gateway treatments to signify the entrance of a special district or area.





Public art and wayfinding signage can be incorporated in the central island to direct traffic and call attention to a place as distinct and different from its surroundings serving as a unique gateway entry.

Roundabouts can cause challenges for pedestrian mobility which should be considered and resolved when designing the new roundabout at Weddington and Concord Farms Roads. The addition of multi-use path along Weddington Road also makes pedestrian and bike safety important. Image on Pg. 65 shows an example how to resolve pedestrian crossings at roundabouts.

Concord Farms Road Re-Alignment

Concord Farms Road is a two-lane road primarily intended to provide access to adjacent agriculture activity on the east and west sides of the road. The road is approximately 20 feet in width with two, 9-foot travel lanes and connects to Weddington Road to the north and Concord Parkway (US 29) to the south. The NCDOT 2018 average daily traffic count map indicates a recorded volume of 5,700 vpd on Concord Farms Road.

Re-Alignment of Concord Farms Road will provide better connection from Weddington Road to Concord Parkway (US 29) and development beyond US 29. The new Concord Farms Road would start north above Weddington road on the Propst property, extend through the new roundabout at Weddington Road and travel south toward the existing cell phone tower. Before reaching the tower, the new road would break from the existing road. Due to an existing stream located on the Christy property this road will have to cross the stream with a bridge at a 90° angle and meet US 29 at a 90° angle as well. Possible cross sections of the New Concord Farms Road are shown on the following page.

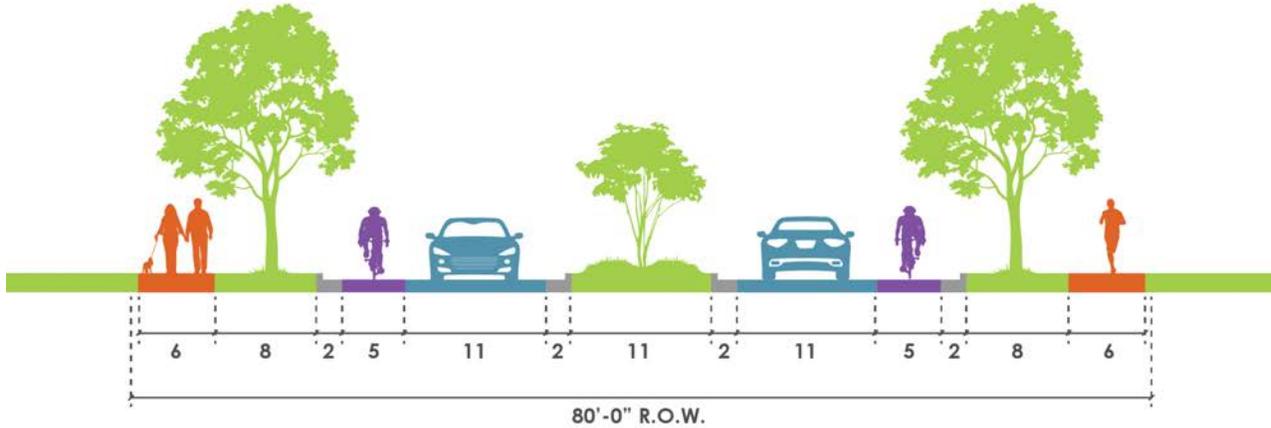
South of US 29 the Concord Farms Road extension could be developed one of three ways as shown in the Map on Page 69. Flexibility in road alignment is provided as to respond to the future development in this area.

Intersection at Concord Farms Road & Concord Parkway (US 29)

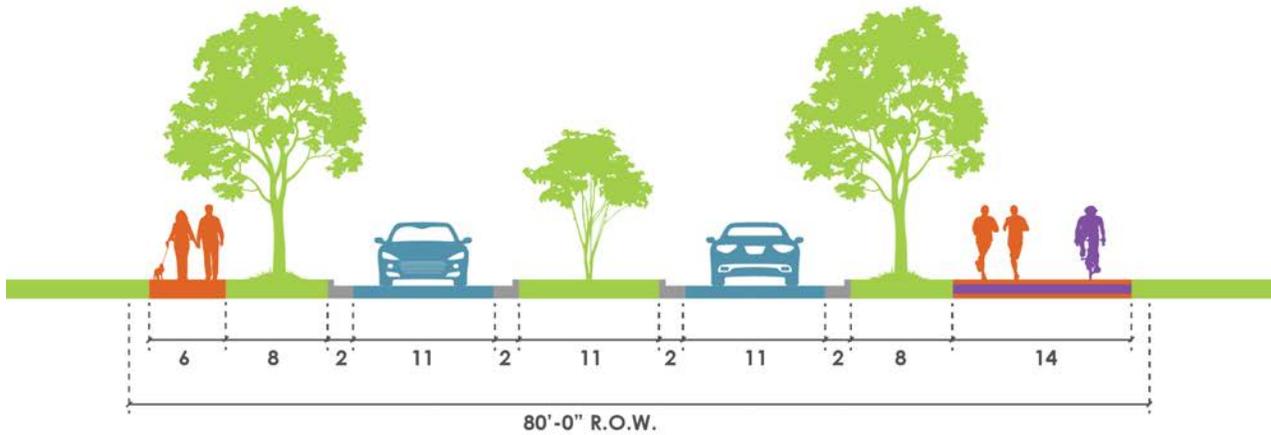
With 24,000 vehicles per day on Concord Parkway (US 29) it is the most highly trafficked road in this area which makes intersections more challenging. This plan recommends a signalized intersection at the new Concord Farms Road Alignment and Concord Parkway (US 29) junction to make pedestrian

CONCORD FARMS ROAD CROSS SECTION OPTIONS

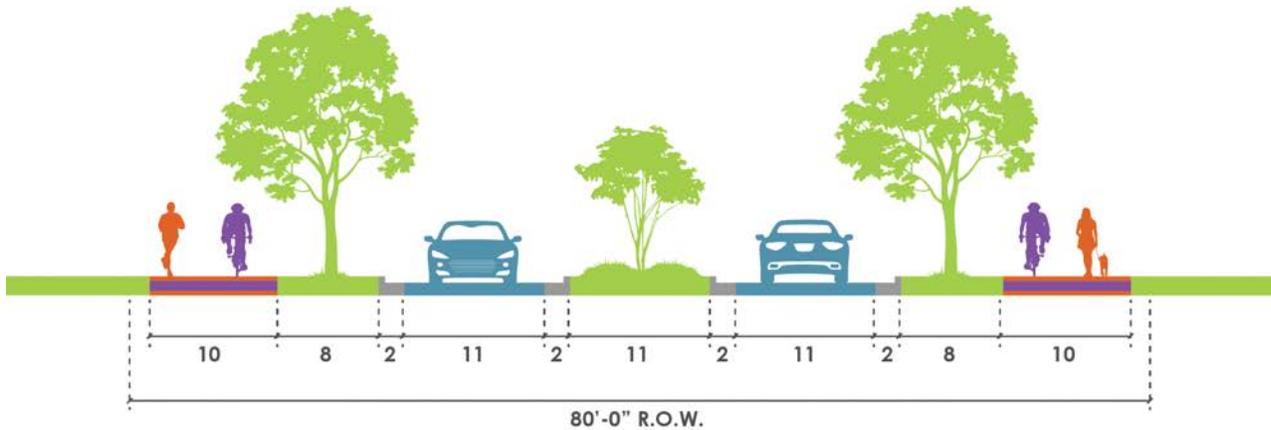
2 Lane Divided (11' Raised Median) w/ Curb & Gutter, Bike Lanes and Sidewalks Both Sides



2 Lane Divided (11' Raised Median) w/ Curb & Gutter and 14' Multi-Use Path One Side



2 Lane Divided (11' Raised Median) w/ Curb & Gutter and 10' Multi-Use Path Both Sides



SIDEWALK
 BIKE FACILITY
 ROADWAY

crossing easier and to accommodate future High Capacity Transit along Concord Parkway (US 29).

George W. Liles Parkway & Site Access

The Transportation Recommendations Map on the following page identifies an optimum location for a full movement intersection on the future section of George W. Liles Parkway located at the midpoint between Weddington Road and US 29. In addition, the Plan identifies optimum locations for additional right-in, right-out access points along this corridor. By establishing these locations early in the process, the overall area circulation pattern can be set. This early decision making will ensure a balance between the mobility needs of the region and the degree of access necessary to accommodate future development. Consistency with the NCDOT driveway manual and access management policy should be consulted prior to the issuance of any permits along this future roadway.

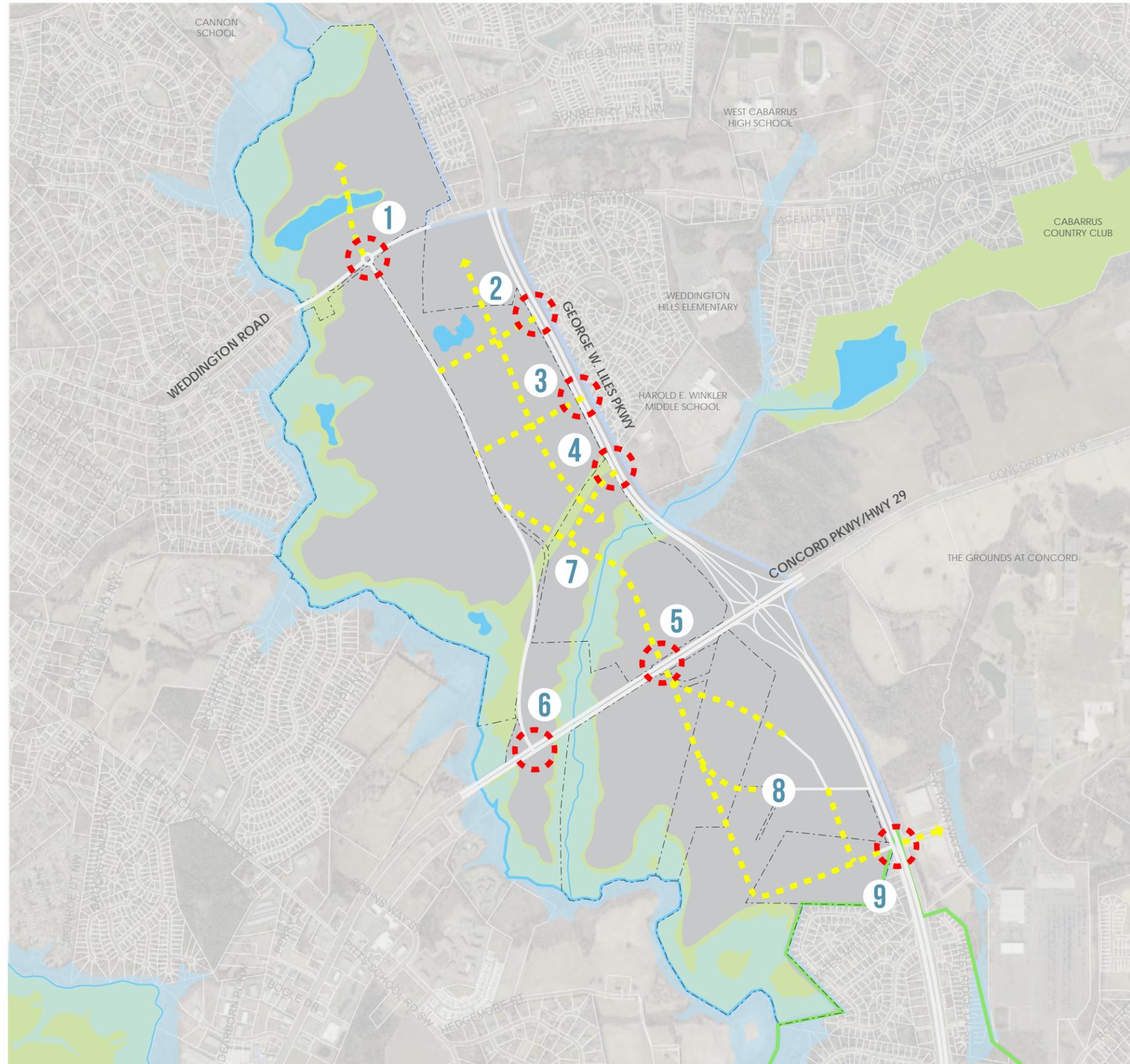
Additional external access will occur along the Weddington Road and US 29 (Concord Parkway) corridors. In order to curtail the potential for a strip pattern of development, access to these roadways should be limited to public street-type access with limited use of private driveways. Given the presence of large parcels along these frontages, internal site access can easily be achieved through responsible design. Right-in, right-out access points should be encouraged as well as directional cross-overs where public street access isn't able to be achieved.

Traffic Impact Analysis (TIA) should be required in order to understand the incremental impacts of development as it occurs and to encourage connectivity and internal site circulation. The small area plan study area is largely a flat undeveloped greenfield site with few constraints and yet there are few if any opportunities for connectivity to the local neighborhood street network. This is in large part due to the lack of adjacent street stubs and the presence of a creek on the western edge of the area.

While external connectivity will in large part be limited to Weddington Road, George W. Liles Pkwy and US 29, there is an opportunity to promote internal mobility through a well-designed and coordinated transportation network. A rigid grid system of streets is not a prerequisite for this to occur; however, the orderly creation of blocks that range between 400 and 1,000 feet can successfully provide for enhanced connectivity within the future development.

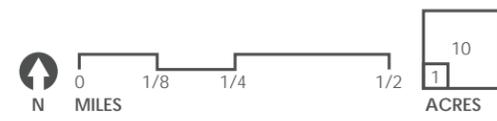


MAP 2.12: AREA A - TRANSPORTATION RECOMMENDATIONS



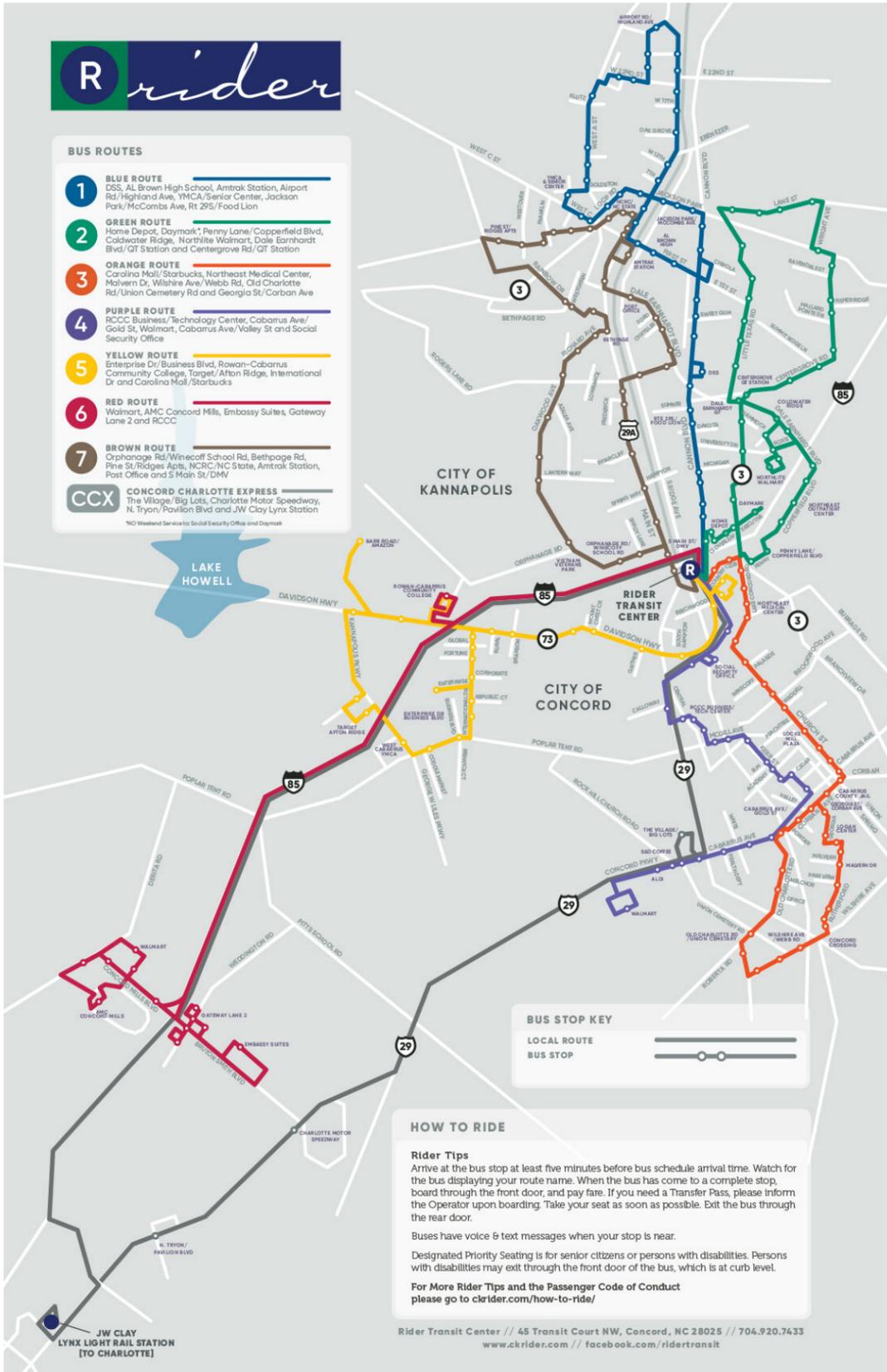
- 1 Roundabout
- 2 Right In Right Out Only
- 3 Full Movement Intersection
- 4 Right In Right Out Only
- 5 Concord Farms Road Realignment w/ Signalized Intersection
- 6 Right In Right Out Only
- 7 Concord Farms Road Realignment to HWY 29
- 8 Concord Farms Road Connection Options
- 9 Full Movement Intersection

- Green Space
- Water
- 100 Yr Floodplain
- Significant Intersections
- New Road Network



MAP 2.13: RIDER TRANSIT BUS SYSTEM MAP

Existing Rider Transit Bus Route System Map



(THIS PAGE INTENTIONALLY LEFT BLANK)

2.3.3. PUBLIC TRANSIT

The following section summarizes the Long Range Public Transit Plan as it pertains to the George W. Liles Parkway Small Area Plan Study Area A.

Rider Transit - 2020 Cabarrus County Long Range Public Transit Plan (LRTP)

Cabarrus County Transportation Service (CCTS) and Concord Kannapolis Area Transportation (Rider) initiated a Long Range Public Transportation Master Plan with the intent of creating a strong vision for improved public transportation throughout Cabarrus County to address needs not only today, but far into the future.

The Concord Urbanized Area (UZA), is adjacent and just north of neighboring Charlotte, North Carolina, in one of the fastest growing urbanized regions in the United States. With this growth comes many opportunities – and many challenges. Regional proximity and activity, coupled with constant population gains, make the communities in Concord and surrounding Cabarrus County significant players in regional growth and transit connectivity. These transit systems and networks are becoming increasingly important to the sustained growth of a region.

At the core of the transit plan was a robust public involvement process that guided the development of the study and helped the study team define the priorities for the plan. This information, along with demographic and market analysis were considered when crafting the plan’s recommendations. It is an ambitious plan that provides recommendations for the next 20 years and defines operational, capital, and personnel improvements in six phases, see Figure 2.3.

Phase 3 Service Recommendations (Year 7-8)

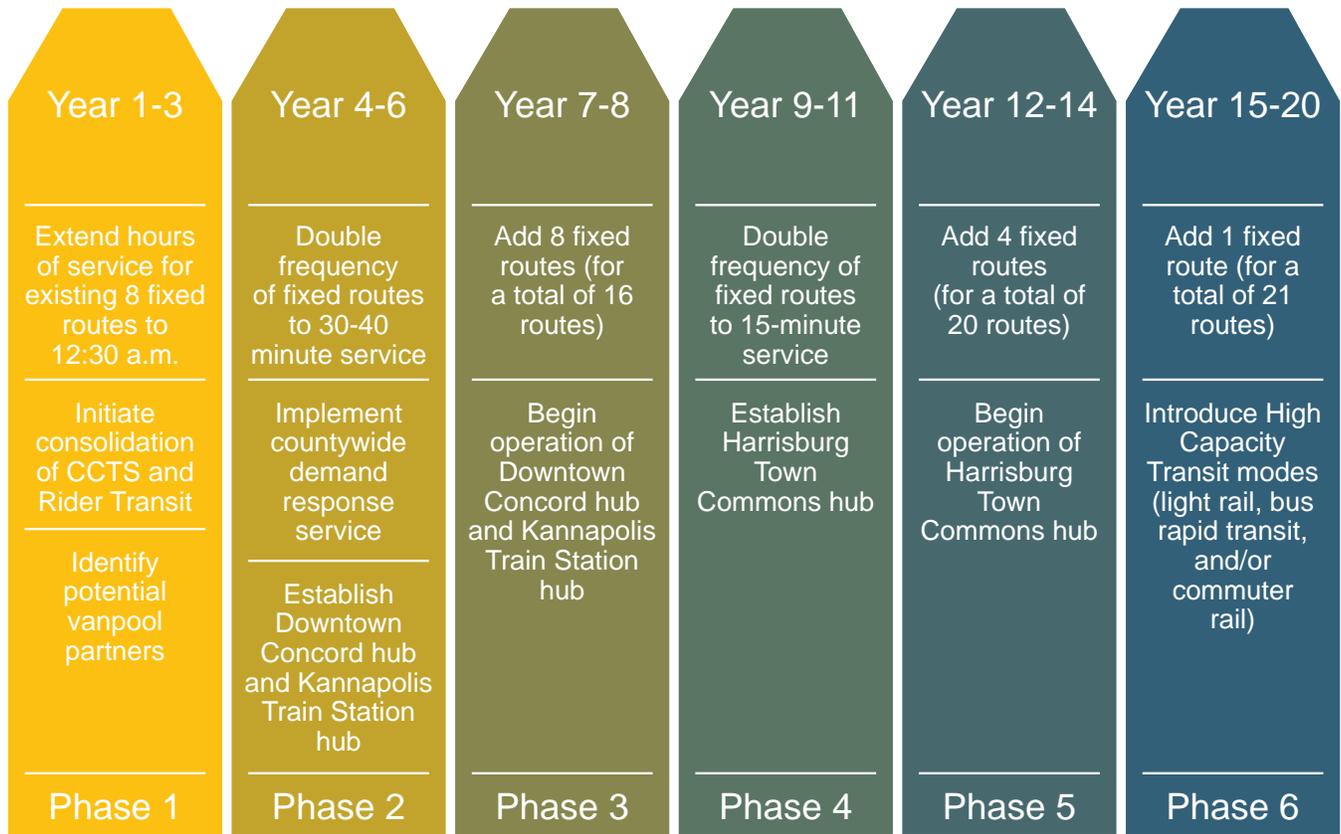
In Phase 3, the Downtown Concord hub would be operational. This new hub, in addition to the Kannapolis Train Station hub, would provide support service for the recommended eight new routes. Two of these routes directly affect the George W. Liles Parkway and US 29 corridors.

Route 302 – George W. Liles Parkway

In order to address the growing transit need along the George W. Liles corridor, Route 302 is recommended to serve the area, which currently has very limited Fixed Route service. Route 302 would depart from the Downtown Concord hub, following Cabarrus Avenue to Concord Parkway,



FIGURE 2.3: RIDER TRANSIT 20 YEAR PLAN - PHASED SERVICE RECOMMENDATIONS



...serving the Grounds at Concord, and then to George W. Liles Parkway. Route 302 would connect with multiple routes and turn around at the Afton Ridge Target. This route would be a bidirectional route and would also serve the Walmart on Concord Parkway. It is recommended, as part of the George W. Liles Pkwy Small Area Plan that Route 302 be modified to follow Concord Farms Road connecting from US 29 to Weddington Road and then George W. Liles Parkway.

Concord Charlotte Express (CCX)

The CCX operates in a loop fashion using US 29 and I-85 currently and in Phase 1 of the LRTP. This routing allows the CCX to operate on an already tight 60-minute cycle, but as a result does not serve any additional stops except the Rider Transit Center and J.W. Clay Light Rail Station on both outbound and inbound trips. In Phase 3, it is recommended that the CCX be aligned to function as a bi-directional route on US 29 and operate as a local route serving the same stops on outbound and inbound trips as route 208. This recommended route would operate on a 90-minute cycle, thereby requiring three vehicles to achieve 30-minute frequency.

For a map of the CCX and Route 302 please see Appendix D.

Phase 4 Service Recommendations (Year 9-11)

In Phase 4, the vehicle headway or frequency would increase to 15 minutes on fixed routes, Monday through Friday and continue on a 60-minute frequency on Saturday and Sunday. Increase in frequency to 15 minutes on all routes would enhance the level of service and provide additional

opportunities for current riders and new riders alike to use the system and become a feasible mobility option for all. By increasing frequency, the agency would make transit more convenient to riders and a more competitive mode with automobiles. Service recommendation changes: Span of service for CCX increases from 4:30 am to 2:30 am 7 days per week.

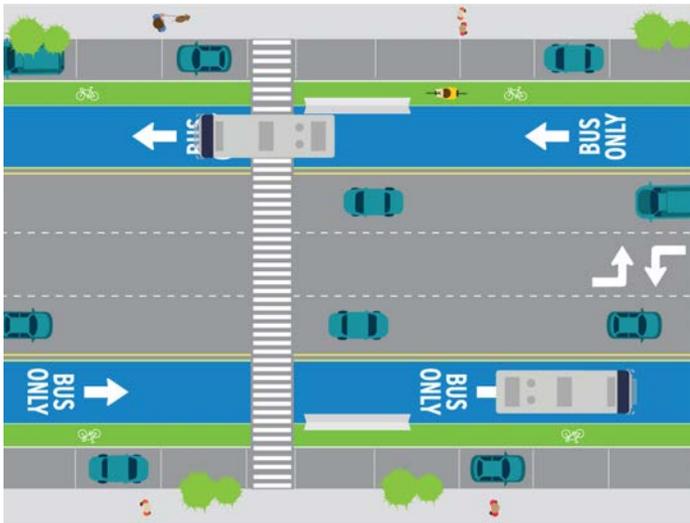
Phase 6 Service Recommendations (Year 15-20)

The most prominent service improvement in Phase 6 is the introduction of high capacity transit (HCT), which could include bus rapid transit (BRT), light rail transit (LRT), and/or commuter rail.

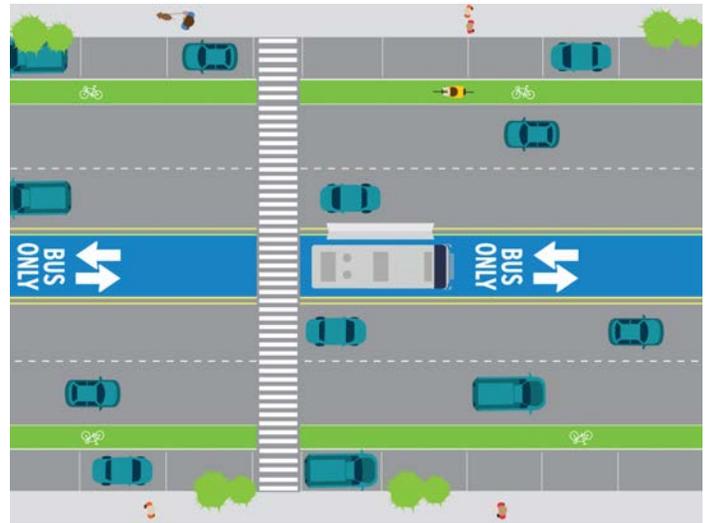
High Capacity Transit (HTC)

Through the stakeholder and public engagement process for the Transit Plan, the following corridors have been identified for potential HTC service:

- HTC-1: Extension from J.W. Clay Light Rail Station on US 29 to current and future development at The Grounds at Concord.
- HTC-2: Extension from J.W. Clay Light Rail Station to exit 49 corridor serving Concord Mills and Concord-Padgett Regional Airport.
- Commuter Rail: North Carolina Railroad Corridor from the Charlotte Amtrak Station to future stations in Harrisburg and Concord as well as the existing Amtrak Station in Kannapolis.



▲ Bus Rapid Transit Corridor-Based Example



▲ Bus Rapid Transit Fixed Guideway Example

BRT Corridor-based

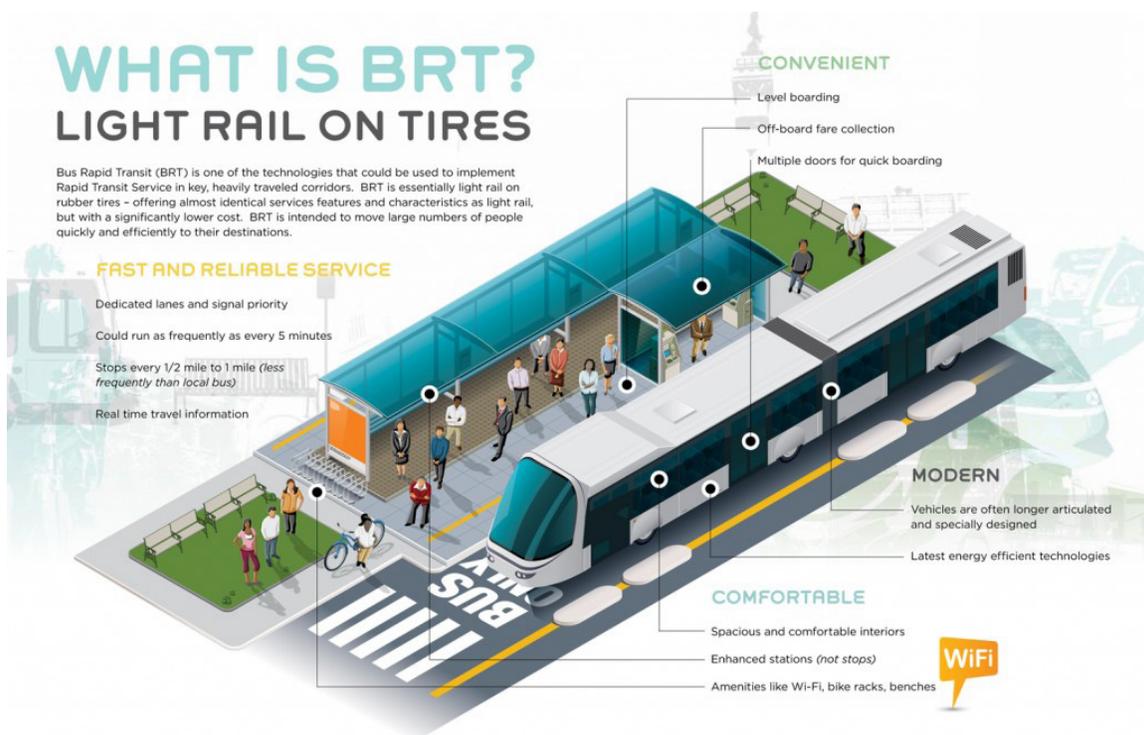
- Separated right-of-way not required for entirety of corridor
- Makes a substantial investment in a specific corridor
- Defined stations
- Traffic signal priority for buses
- Short headway times
- Bidirectional services for a substantial part of weekdays

BRT Fixed Guideway

- Majority of project operates in a separated right-of-way dedicated for public transportation use
- Makes a substantial investment in a single route within a defined corridor
- Defined stations
- Traffic signal priority for buses
- Short headway times
- Bidirectional services for a substantial part of weekdays & weekend days

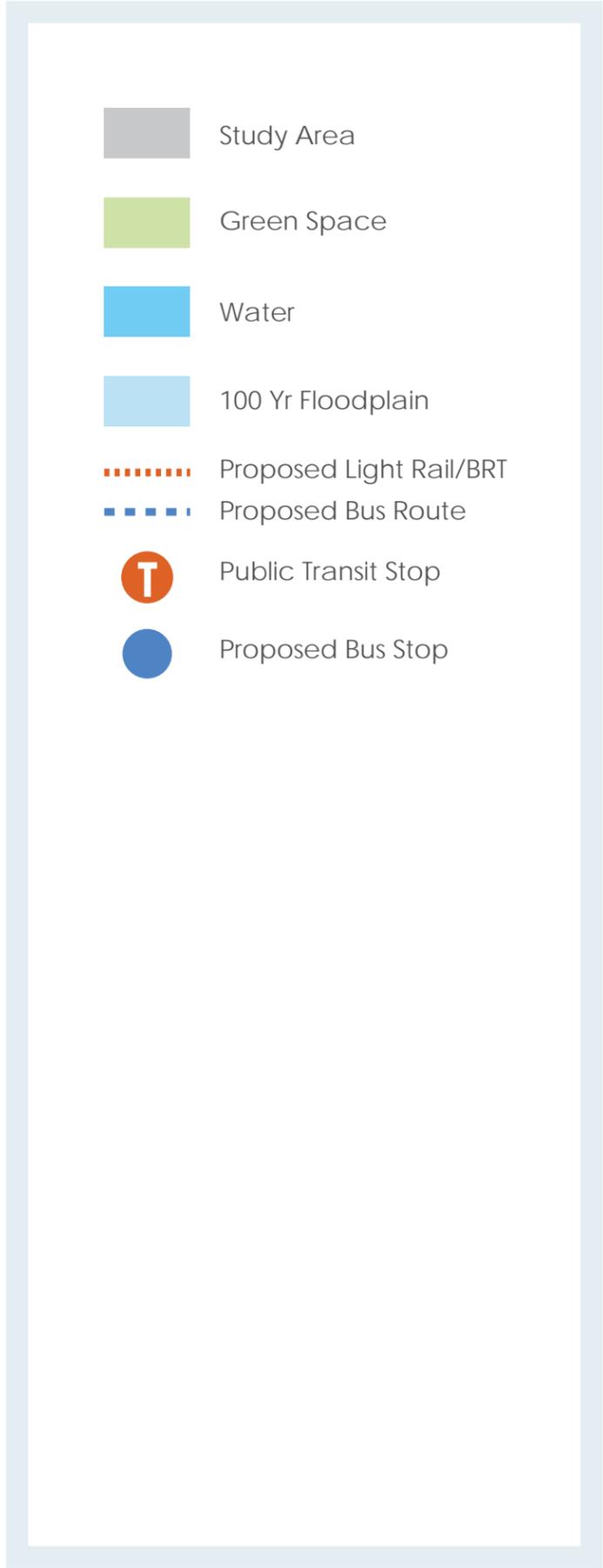
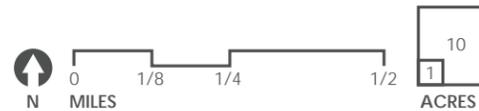
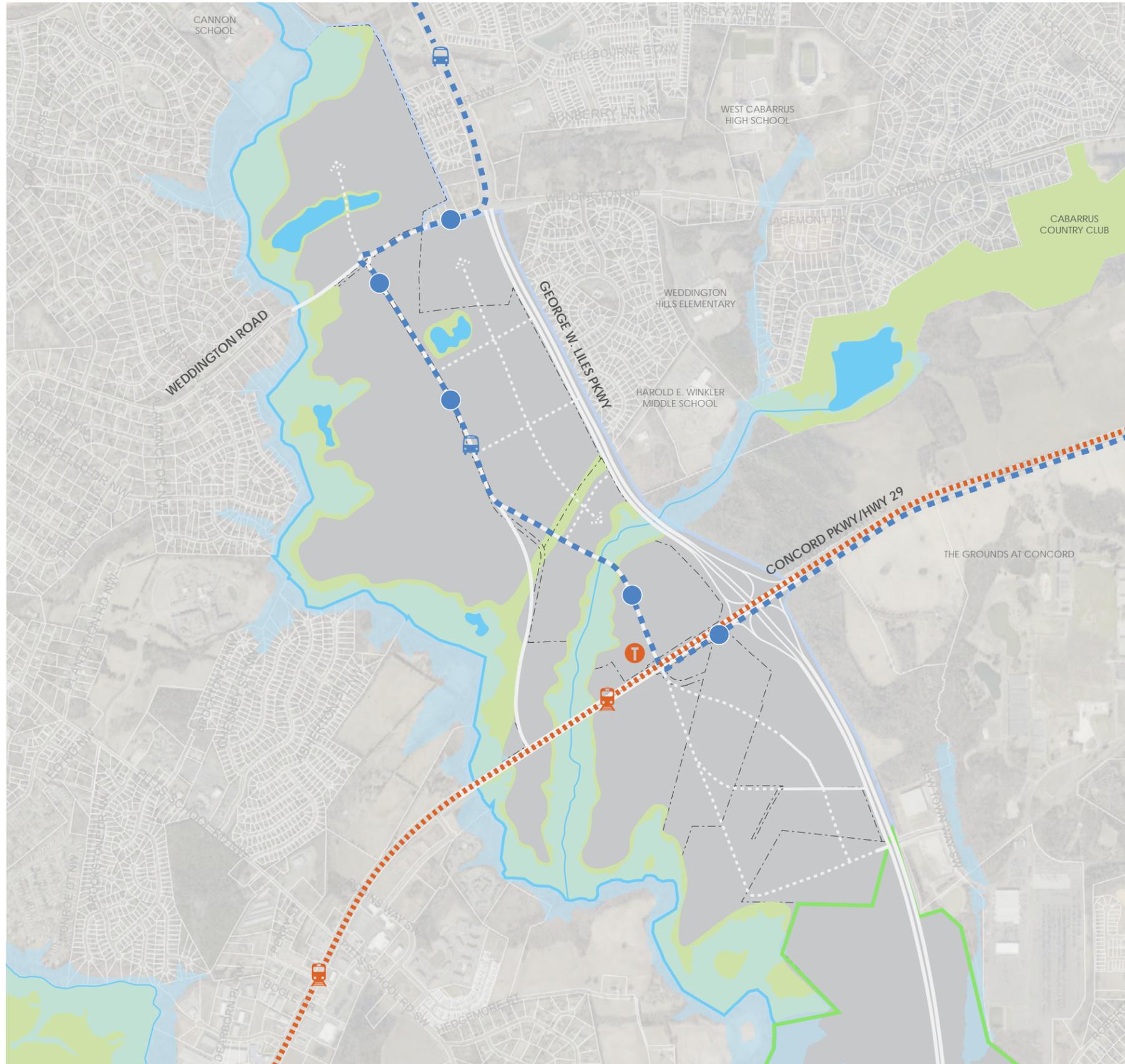
Of the three potential HCT services HCT-1 has the most impact on the George W. Liles Pkwy Small Area Plan. In this corridor the potential transit modes are bus rapid transit (BRT) or light rail transit (LRT). BRT in a dedicated guideway is a cost-effective solution that can mimic rail operations, with lower passenger capacity per vehicle, but also lower per vehicle cost. Dedicated roadway for bus can be installed cheaper and faster than rail. Corridor-based BRT projects do not require separated right-of-way for most of the corridor unlike fixed guideway LRT projects. In contrast, LRT technology can couple multiple vehicles to carry greater passenger volumes when compared to BRT. Vehicle length and conflicts with auto traffic may limit service ability to operate in the densest areas, but it can contribute to potential economic development. LRT has higher capital and annual operation costs due to the rail guideway and electrified infrastructure systems.

The specific transit mode(s) and alignments would be determined during feasibility studies that would consider the following factors, among others: ridership forecasts, capital costs, and operating and maintenance costs, socioeconomic impacts, environmental impacts, and traffic impacts. The feasibility studies are recommended to be conducted during Phase 1 of the LRTP.



Indy Connect Graphic, "What is BRT?", <https://indyconnect.org/the-central-indiana-transit-plan/about-bus-rapid-transit-brt/>

MAP 2.15: AREA A - PUBLIC TRANSIT RECOMMENDATIONS



(THIS PAGE INTENTIONALLY LEFT BLANK)

2.3.4. PARKS & RECREATION

Parks & Recreation - Open Space Connectivity Analysis Plan

Adopted in May 2019, the purpose of the Open Space Connectivity Analysis (OSCA) Plan is to identify feasible, constructible bicycle and pedestrian routes along stream corridors, through open spaces, and along roads to create a more connected Concord. The City of Concord elected to prepare an open space connectivity analysis, expanding upon the Greenway Master Plan included in the Parks and Recreation Master Plan as adopted by City Council in January 2017. The OSCA is a tool that City Staff can use as a guide to approve, plan, and ultimately build the facilities needed to connect residents and guests of Concord.

The following section summarizes the Open Space Connectivity Analysis Plan as it pertains to the George W. Liles Parkway Small Area Plan Study Area A.

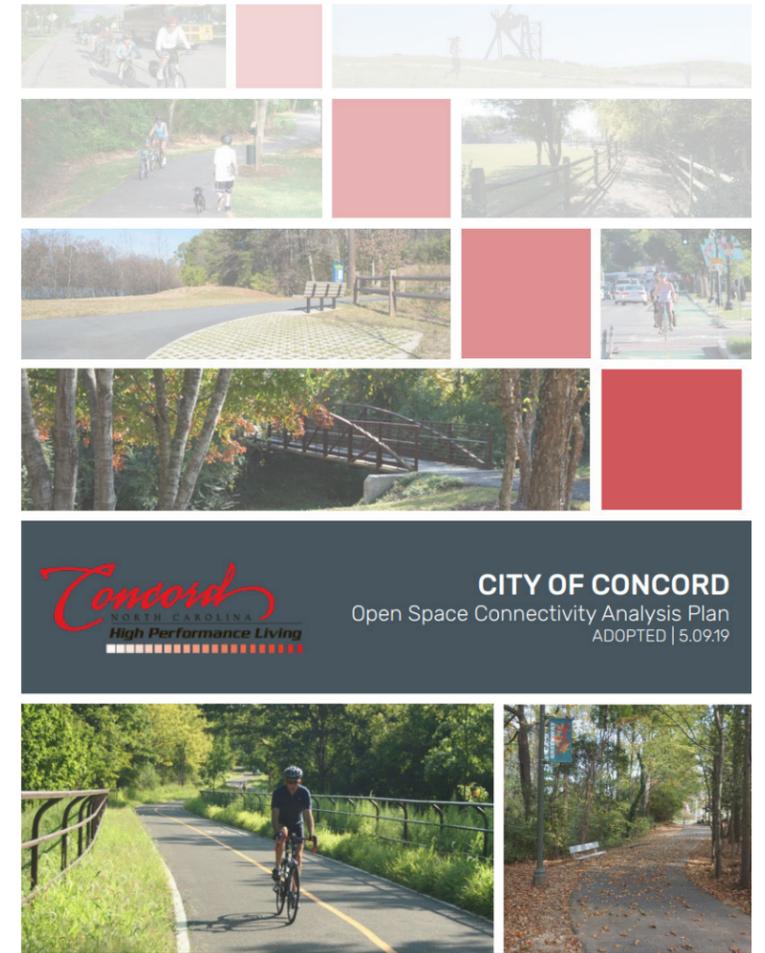
Greenways & Multi-Use Paths

Coddle Creek Greenway

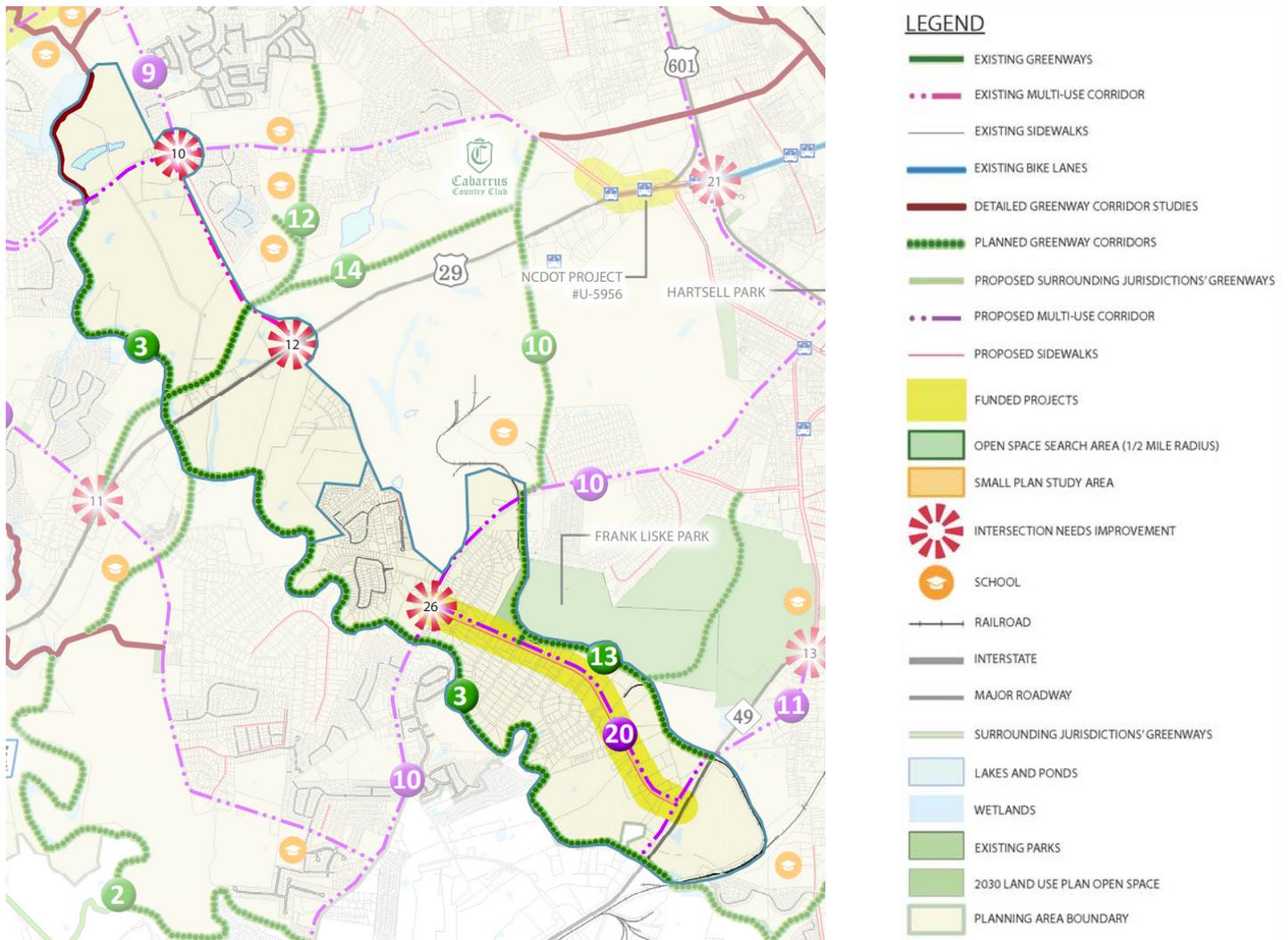
Phase I & II, planned for a 0-5 year time frame in the Open Space Connectivity Analysis Plan, starts at Afton Village and continues to Weddington Road. Phase III and beyond are in the 5+ year time frame.

Phase I: Dorton Park/Afton Village to Poplar Crossing Drive

This phase of Coddle Creek Greenway creates a loop within the Afton Village community, tying into Dorton Park. There are also connections to the West Cabarrus YMCA, Cannon School, Poplar Crossing Commons, Cabarrus Charter Academy, and a future connection under I-85 to Afton Ridge in Kannapolis. This phase utilizes several existing on-street and sidewalk connections within Afton Village to complete the phase, thus reducing the overall costs for construction and potential right of way acquisition needs. Off-street, multi-use paths may be incorporated but would require additional study, community input, and cost. With this project, stream restoration of Afton Run Branch is possible.



MAP 2.14: CONCORD CONNECTIVITY PLAN - GEORGE W. LILES PKWY PLAN BOUNDARIES



This map is cropped from the Parks & Recreation Open Space Connectivity Analysis Plan's Connectivity System Map.

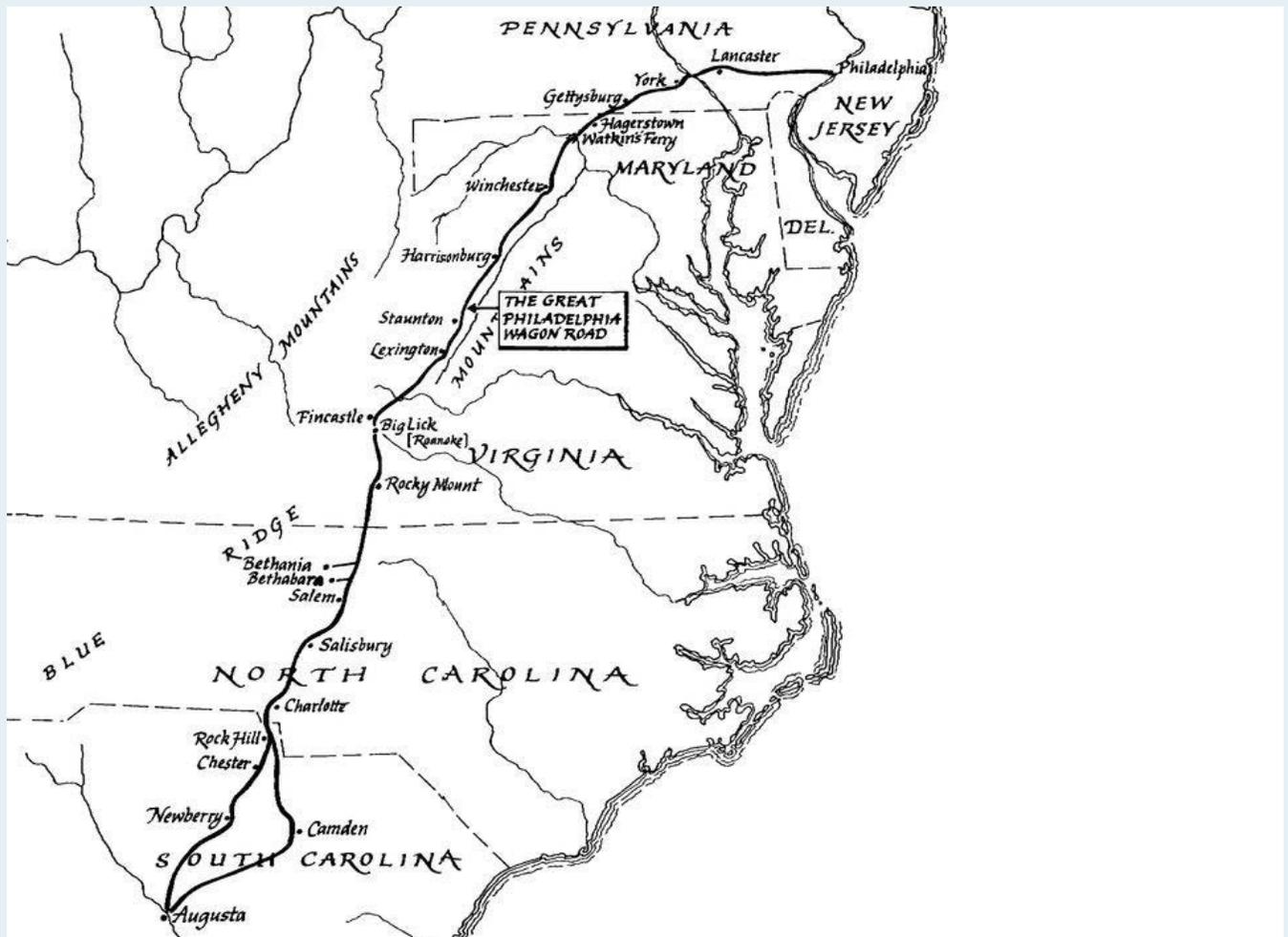
Phase II: Poplar Crossing to Weddington Road

The second phase of the Coddle Creek Greenway extends Coddle Creek Greenway Phase 1 from Poplar Crossing to Weddington Road where a new trailhead will be built as part of the project. This beautiful section of creek connects several schools and communities. A multi-use path on the south side of Weddington - will eventually tie the Coddle Creek Greenway to the new West Cabarrus High School.

Great Wagon Road Greenway

The previous 2005 Concord Pkwy/Roberta Church Road Area Plan established this trail alignment as being preserved and celebrated within the site, especially due to its proximity to the existing Middle and Elementary School and new West Cabarrus High School. This plan recommends the installation of a 10 to 14-foot wide greenway trail with planted sides and interpretive signage along its path. This greenway trail would tie into Coddle Creek, the greenway trail that meanders through the study area on the western side of the site.

THE GREAT WAGON ROAD - FROM PHILADELPHIA TO THE SOUTH



For nearly 150 years after North America was settled, it remained a green wilderness. Only a few trails cut through the vast forests which spread from New Hampshire to Georgia, for the Appalachian Mountains thrust a stern barrier between the Atlantic plateau and the unknown interior of the continent.

As settlers moved inland, they usually followed the paths over which Indians had hunted and traded. Many of these trails had been worn down in earlier ages by buffalo, which once roamed the eastern uplands in search of grazing lands. These paths usually followed valleys and river shores.

Few trails in early America were more important than the Indian route which extended east of the Appalachians from Pennsylvania to Georgia. This ancient Warriors' Path was long used by Iroquois tribesman of the north to come south and trade or make war in Virginia and the Carolinas. Then, by a series of treaties with the powerful Five Nations of the Iroquois, the English acquired the use of the Warriors' Path. After 1744, they took over the land itself.

The growth of the route after 1744 into the principal highway of the colonial back country is an important chapter in the development of a nation. Over this Great Philadelphia Wagon Road,

vast numbers of English, Scotch-Irish, and Germanic settlers entered this continent and claimed lands.

The endless procession of new settlers, Indian traders, soldiers, and missionaries swelled as the Revolution approached. "In the last sixteen years of the colonial era," wrote the historian Carl Bridenbaugh, "southbound traffic along the Great Philadelphia Wagon Road was numbered in tens of thousands; it was the most heavily traveled road in all America and must have had more vehicles jolting along its rough and tortuous way than all other main roads put together."

As the principal highway of the eighteenth-century frontier southward from Pennsylvania, the Wagon Road also played an important part in the French and Indian wars and in the American Revolution. Daniel Boone and Davy Crockett traveled it as explorers. George Washington knew it as an Indian fighter. Countless soldiers - Andrew Jackson, Andrew Pickens, John Sevier, Andrew Lewis, Francis Marion, Lighthorse Harry Lee, Daniel Morgan, and George Rogers Clark among them - fought over it.

When British forces captured Philadelphia early in the Revolution, the Continental Congress escaped and fled down the great Wagon Road to York. Cornwallis and his troops traveled the Wagon Road in their attempt to neutralize the southern colonies. Many important battles were fought on or near the Road which became the War's western front: Kings Mountain, the Cowpens, Gilford Courthouse, Salisbury, and Camden were some of them.

From the Great Wagon Road, pioneers passed through Cumberland Gap and the Holston River Settlements into the territories which became Kentucky and Tennessee. This route, which Daniel Boone opened in 1775, became an umbilical cord by which the first sizable trans-Appalachian settlements were nurtured to statehood. Over this Wilderness Road went Henry Clay and the forebears of Abraham Lincoln, among countless others.

The chronicle of the Wagon Road is the chronicle of infant America, from 1607 until the age of the railway. It is the story of achievement against great odds. Breaking with European traditions which they brought to America with them, the diverse settlers along the Wagon Road began to create the new American society which changed the nineteenth-century history of the world.¹



¹ Parke Rouse, Jr., "The Great Wagon Road - From Philadelphia to the South," Richmond: The Dietz Press; 1915. Prologue.

Parks & Other Facilities

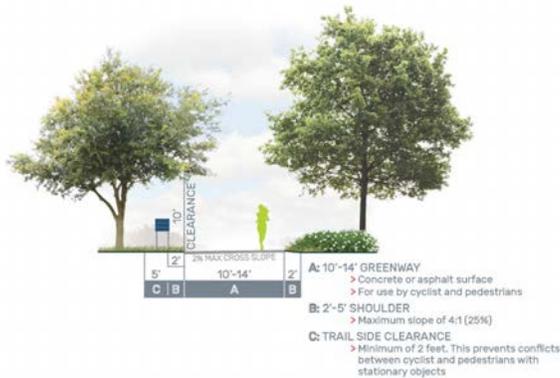
Trailheads

Trailheads are designated public access points to the greenway trail system, connecting roadways and/or activity centers to the greenway system and may include amenities such as:

- Vehicle and bicycle parking
- Restrooms
- Seating areas/benches
- Shelters and picnic areas
- Drinking fountains
- Trash and recycle receptacles
- Bike share stations
- Pet waste stations
- Bicycle repair stations
- Public art
- Lighting
- Wayfinding and informational signage



OSCA | 8'-10' Benched Greenway with 2' Shoulder



OSCA | 10'-14' Greenway with 2'-5' Shoulder



OSCA | 10'-14' Multi-Use Path with 2' Shoulder

Consider locating trailheads in conjunction with other public facilities or through a shared use agreement with owners of adjacent parking areas. Trailheads could be classified into major and minor categories.

Major trailheads should be established near high population and high use areas such as large residential and commercial developments, transportation nodes, or popular parks. Such siting makes the trailhead accessible to a larger number of users and provides greater access to the greenway trail system.

- Major trailheads can have a large paved parking lot with emergency and maintenance vehicle access and turnaround. ADA accessible parking spaces must be provided near the site's accessible route, at a rate of one accessible space per 25 standard spaces.
- Consider one-way vehicle circulation within parking areas to minimize road width.
- Provide user access from local streets when major trailheads are located near neighborhoods.

Bike Facilities

In North Carolina, the bicycle has the legal status of a vehicle. Cyclists have full rights and responsibilities when on the road and are subject to the same rules and regulations that govern the

operation of a vehicle. When riding on the road, cyclists must ride on the right and in the same direction as traffic. All traffic signs and signals must be obeyed, and hand signals should be used to communicate intended movements. Bicycles must also be equipped for night riding with the appropriate front lamp and rear reflector. Thus, riding on the road and being treated with the same status as a vehicle can be intimidating for most recreational riders. As such, safer more comfortable provisions should be made with a multi-modal transportation system that caters to the bicycle.

There are various bike facilities that can be accommodated based on existing site conditions. Some are incorporated into a mixed traffic scenario, mixing bicycles with cars in the same space, while other facilities are visually or physically separated from traffic. The definitions and graphics as provided were obtained

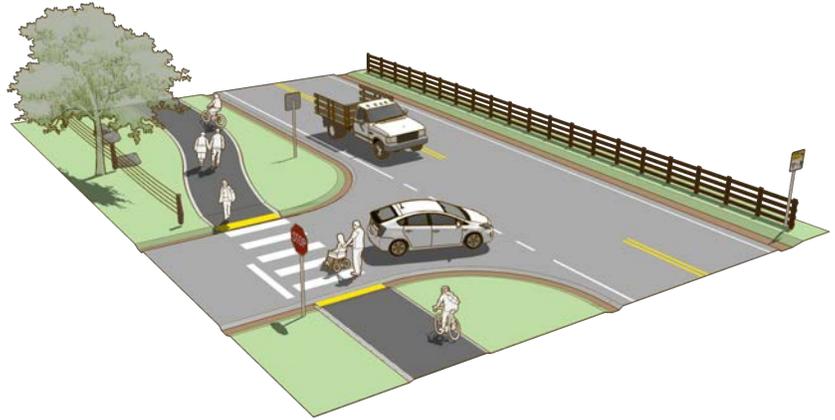
from “Small Town and Rural Design Guide – Facilities for Walking and Biking” and “NACTO Urban Bikeway Design Guide”.

Bike Lanes

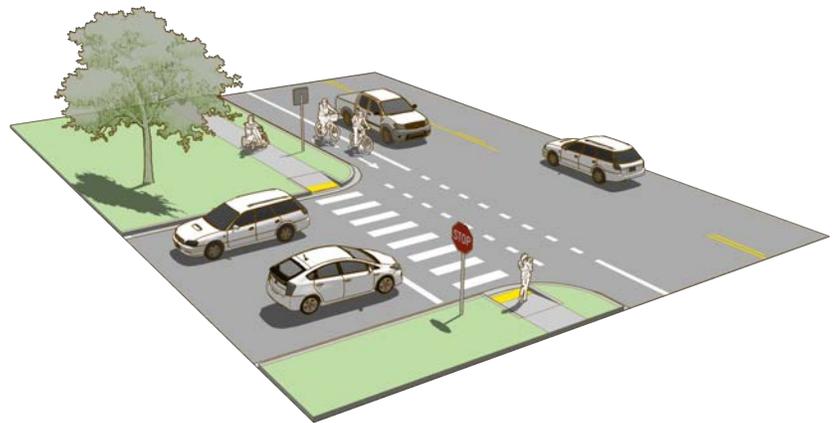
Bike lanes allocate an exclusive space for bicyclists with a designated 5-foot striped lane, pavement markings, and signage and enable bicyclists to ride at their chosen speed without interference from traffic. Conventional bike lanes are located directly adjacent to motor vehicle travel lanes and run curbside when no parking is present or adjacent to parked cars on the right side of the street. They typically follow the same direction as motor vehicle traffic and have no physical barriers (bollards, medians, raised curbs, etc.) that restrict vehicular encroachment into the bike lane.

Benefits of conventional bike lanes include:

- Increases use comfort and confidence on busy streets.
- Creates separation between bicyclists and automobiles.
- Increases predictability of bicyclist and motorist movement and interaction.
- Increases streets' carrying capacity.
- A visual reinforcement of the bicyclists' right to the street.



Small Town & Rural Design Guide | Physically Separated - Multi-Use Path



Small Town & Rural Design Guide | Physically Separated - Sidewalk

Bike lanes are most conducive on streets with:

- $\geq 3,000$ motor vehicle average daily traffic.
- A posted speed ≥ 25 mph.
- High transit vehicle volume

Buffered Bike Lanes

A Buffered Bike Lane is a conventional bike lane paired with additional buffer space to separate the motor vehicle traffic lane and/or parking lane from the bicyclists. Multiple pavement markings are typically used to delineate the edge of the travel way for both motor vehicles and bicyclists.

Benefits of buffered bike lanes include:

- Provides greater distance between vehicles and bicyclists.
- Provides space for bicyclists to pass other bicyclists without encroaching into adjacent vehicle traffic.
- Encourages bicyclists to ride outside of the door zone when buffer is located between parked cars and the bike lane.
- Provides a greater space for bicycling, but not so great that the bike lane is mistaken for a travel or parking lane.
- Appeals to a wider cross-section of bicycle users.
- Encourages bicycling by contributing to the perception of safety among bicycle network users.

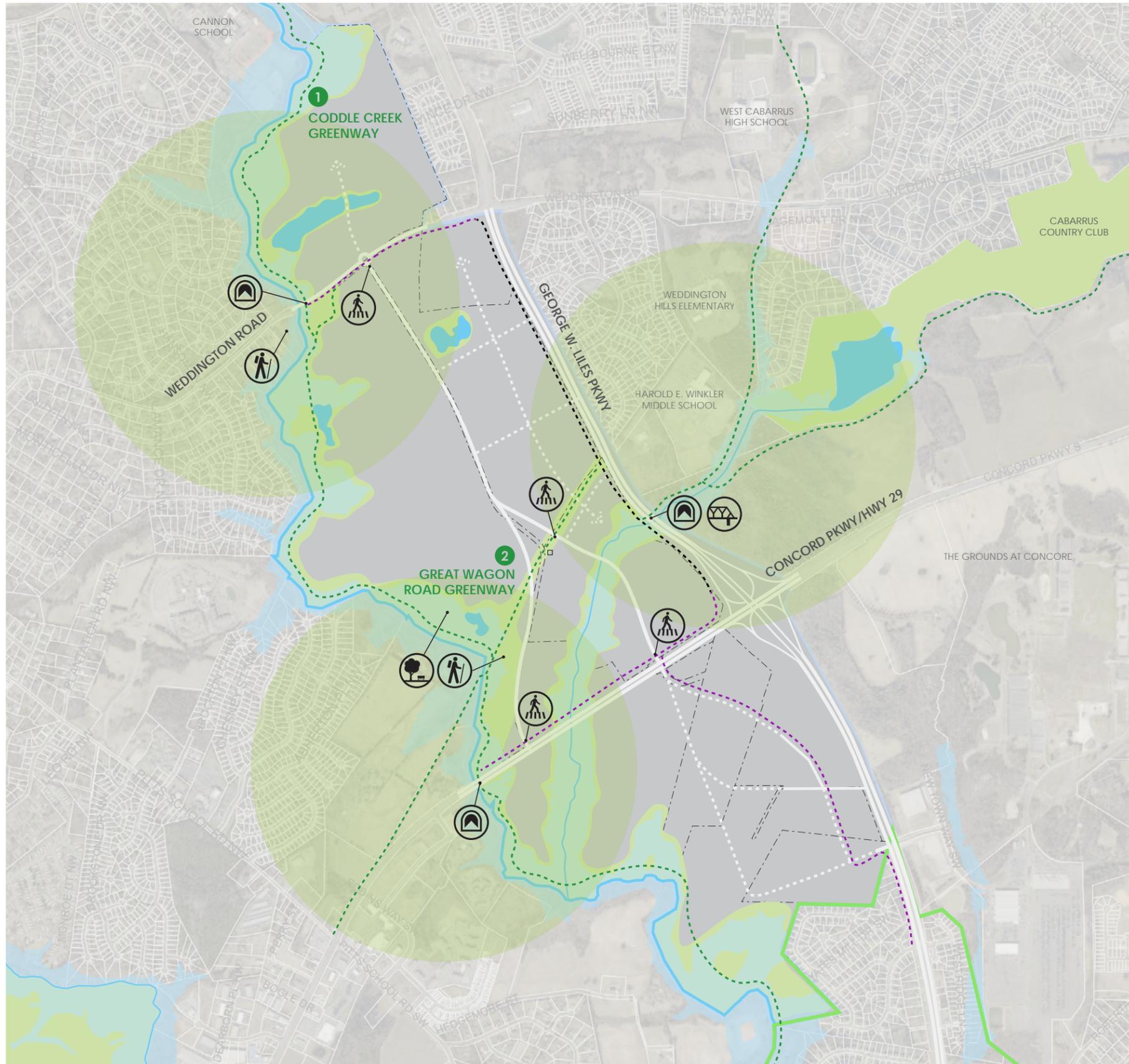
Buffered bike lanes can be incorporated:

- Anywhere a standard bike lane is being considered.
- On streets with high travel speeds, high travel volumes, and/or high amounts of truck traffic.
- On streets with extra lanes or extra lane width.

Pedestrian Sidewalks

Sidewalks are dedicated to and designed for use by pedestrians. They should be safe, comfortable, and accessible to all. Sidewalks are physically separated from the roadway by a curb or unpaved buffer space and are paved. Like multi-use paths, sidewalks are typically parallel to a roadway but are designed for pedestrians only, not for bicycles or other recreational purposes.

MAP 2.16: AREA A - PARKS & RECREATION RECOMMENDATIONS



- Small Area Plan Boundaries
- Open Space
- Water Feature
- Flood Data
- Greenway
- Existing Path
- Proposed Multi-Use Path
- Greenway
- Pedestrian Bridge
- Crosswalk
- Pedestrian Trail
- Trailhead Park
- Trailhead

Coddle Creek Greenway

The Open Space Connectivity Analysis Plan calls for Phase I & II planned for 0-5 year time frame and starts at Afton Village and continues to Weddington Road. Phase III and beyond are in the 5+ year time frame.

Great Wagon Road Greenway

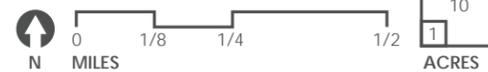
The 2005 Concord Pkwy/Roberta Church Road Area Plan established this trail line as being preserved and celebrated within the site, especially due to its proximity to the elementary school. This plan recommends the installation of a 10 to 14-foot wide greenway trail with planted sides and interpretive signage along its path. This path would tie into Coddle Creek the greenway trail that meanders through the study area on the western side of the site.

Multi-Use Paths

Along Weddington Road & Concord Parkway (US 29) extending from George W. Liles Pkwy to the Coddle Creek Greenway. Connects to the existing Multi-Use Path on George W. Liles to create a loop. Pedestrian Crosswalk added to Concord Farms Road at Weddington Road and Concord Pkwy.

Major Trailhead Park

Major Trailhead Park located near the intersection of the Coddle Cree and Great Wagon Road Greenways. Major trailheads should be established near high population and high use areas such as large residential and commercial developments, transportation nodes, or popular parks. Such siting makes the trailhead accessible to a larger number of users and provides greater access to the greenway trail system.



(THIS PAGE INTENTIONALLY LEFT BLANK)



2.4. AREA A RECOMMENDATIONS

Live-Work-Play

- Lifestyle-focused development around a village center off George W. Liles Pkwy at the intersection with Concord Farms Road.
- Workplace-focused development near the Concord Parkway/George W. Liles Parkway interchange.
- Create a range of housing opportunities and choices throughout the study area.
- Install the Coddle Creek Greenway & Great Wagon Road Greenway as recommended in the OSCA Plan.

Healthy & Active Living

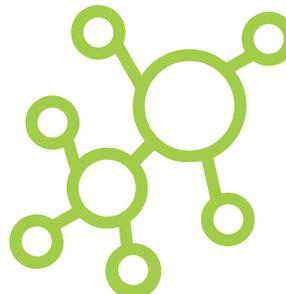
- Encourage opportunities for urban mini-farms (under five acres) and other agriculture uses to provide local fresh produce.
- Encourage Neighborhoods to develop Community Gardens and programmed Farmer's Markets at the village center.
- Develop a Public Park at the intersection of the two Greenways including restrooms and trailhead parking.
- Install bike facilities, parks and greenways to encourage wellness, active living, and healthy commuting.

Connected & Accessible Places

- Install Public Transit Bus Routes and High Capacity Transit (Bus and/or Light Rail) as recommended in the Long-Range Public Transportation Master Plan.
- Provide safe roads, sidewalks and multi-use paths for all modes of transportation from bus to automobile, bicycle, and foot.
- Re-align and extend Concord Farms Road as recommended to encourage multi-modal connections throughout the study area
- Install clear, well-designed wayfinding and street signage.

Enhanced Quality of Life

- Provide easy access from housing to workplace, dining, shopping and recreational activities via all modes of transportation.
- Provide enhanced design quality that encourages healthy and sustainable design features.
- Create high quality amenities like public art, entertainment, and ample green/open space.
- Develop distinct and attractive communities that have a strong sense of place.



2.5. IMPLEMENTATION

2.5.1. TYPES OF IMPLEMENTATION

Regulatory

Regulatory implementation for Study Area A takes two primary forms: Land Use Plan amendments and zoning changes. In regards to the Land Use Plan, the Cochran/Dellinger/Austin and Southern Bootsmead property owners would each need to apply for a land use plan amendment reclassifying their properties from the existing Agricultural land use category to the Suburban Neighborhood land use category.

A rezoning is typically driven by developers and property owners for specific future development projects. Staff will consider this plan and any accompanying guidelines in the staff recommendation and presentation to the Planning and Zoning Commission and/or City Council during the decision-making process.

Public Infrastructure

Public infrastructure investment is typically driven in reaction to demand. However, the City may work with property owners to pursue proactive infrastructure investments with careful consideration and involvement of City Council and City Management. Developers typically assume the expenses of on-site infrastructure improvements and extensions.

Partnerships

Public-private partnerships can take several different forms. Public entities may contribute to the development in the form of acquiring land for public service provision or occupying space in the development. The City has made public infrastructure investments (e.g. extension of sewer lines) in exchange for a portion of land sales following the investment or in place of a more typical tax-based incentive. Synthetic tax increment financing districts are another potential form of public-private partnership. Any details on partnership opportunities would be negotiated between the City and the development team.

2.5.2. KEY VARIABLES AND CONSTRAINTS

Surrounding Development

Existing and future development surrounding the site may influence how properties within Area A develop. The Bootsmead property to the east of George W Liles Pkwy and north of Hwy 29 (adjacent to the Cabarrus County Club) may develop in several different ways. It could maintain its current industrial zoning classification and support new businesses looking to locate in Concord; however, the future land use category is for a Mixed Use Activity Center that allows for residential, commercial, and office uses.

Staff expects the southern side of Hwy 29 (the former Philip Morris Campus) to develop as a substantial employment center, comprehensively referred to as the Grounds at Concord. Significant employee and freight traffic is likely to accompany such development. The exact industries that will make up this employment center are not determined yet. Developers should be sensitive to the way industrial and employment based projects at the Grounds at Concord may interact with new development expected in Study Area A.

Sequencing

Another major variable within Study Area A is in the sequencing — does residential or non-residential development come first? In early planning discussions regarding Area A, staff frequently considered the developments of Kingsley and Baxter in Fort Mill, SC as models for Study Area A. However, the two developments represent two distinct development patterns. Baxter developers followed the more traditional local pattern of residential development preceding the commercial development to build in the customer base for the future commercial tenants. Kingsley developers were driven by a large corporate tenant – LPL Financial – and its specific needs. Residential will most likely develop first in Study Area A, but walkable lifestyle and workplace centers should not lag too far behind the residential development.

Public Investment

Staff expects multi-modal transportation will be the biggest piece of public investment in the development of Study Area A. The City has clear plans for the greenway infrastructure, but the plans for roadway improvements and the realignment of Concord Farms Road are less clear. The implementation of park space, any needed infrastructure improvements, and any public participation in public art for the site would be subject to future negotiation between the development teams and the City.

City investment priorities are based on current Council goals. As of the date of plan creation, the City Council is interested in the creation of affordable housing and an employment base. Projects including components that address these priorities may attract City Council's investment of public funds in the project. Again, City Council goals are subject to change over time. The City does not commit to any investment without City Council's approval.

Infrastructure

As the community continues to build out, developments may test or strain the capacity of existing City and State infrastructure (e.g. roads, water, sewer, electric, etc.). There may come a time when project demands exceed the infrastructure service capacity. Projects within the study area may need to delay until infrastructure can be developed to serve the demand. The public may perceive congested roadways or other infrastructure failures prior to a traffic impact analysis or other technical indicators concluding that the infrastructure has exceeded its capacity. This perception may complicate any project that requires City Council approval.

PART 3: STUDY AREA B

3.1. AREA B PLAN OVERVIEW

3.2. EXISTING CONDITIONS

- 3.2.1. Existing Development
- 3.2.2. Existing Infrastructure
- 3.2.3. Existing Zoning
- 3.2.4. 2030 Land Use Plan

3.3. FUTURE FOCUS

- 3.3.1. Land Use & Built Form
- 3.3.2. Transportation
- 3.3.3. Public Transit
- 3.3.4. Parks & Recreation

3.4. AREA B RECOMMENDATIONS

3.5. IMPLEMENTATION

Area B

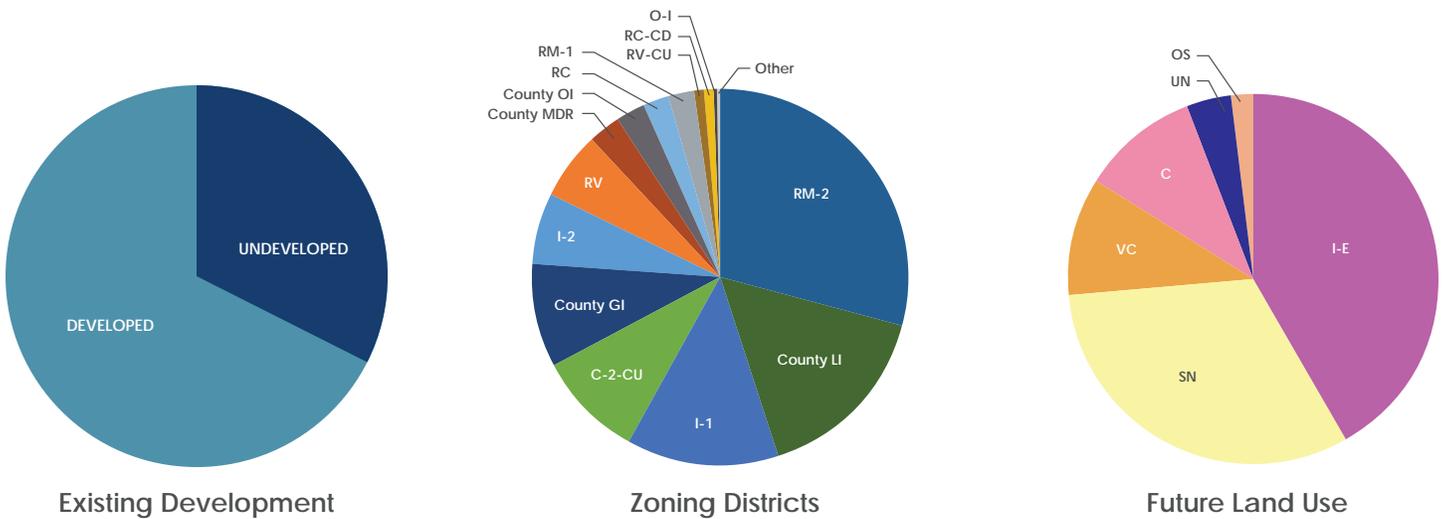


3.1. AREA B PLAN OVERVIEW

Live-Work-Play

Live-work play communities are rooted in the idea that people are happier living in neighborhoods that provide for most of their needs. A live-work-play community has a variety of housing, is close to local workplace options where residents of the community work, and provides places for recreation, shopping, eating and entertainment. Live-work-play communities provide benefits such as convenience for low-stress living, more walking, a healthier lifestyle, and greater connection with neighbors.

- New village center development near the George W. Liles Pkwy/Stough Road and Roberta Road Intersection.
- Encourage small scale industrial workplace opportunities along George W. Liles Pkwy extension near the existing Motorsports Industrial Park.
- Build upon the existing mix of housing opportunities throughout the study area.
- Install the Coddle Creek Greenway as recommended in the Open Space Connectivity Analysis Plan & improve connections to Frank Liske Park.



Healthy & Active Living

Healthy and active living choices, beyond their natural habits, involve the provision of infrastructure and amenities that encourage healthy activity and access to healthy food. In a general study of active living infrastructure, indicators may include miles of bike lanes, sidewalks, greenways and amount of active open space, while access to healthy food may be measured by proximity of a population to grocery stores. These amenities are among the top desired to drive demand for both housing and workspace development and will continue to be important in the post COVID-19 recovery. The George W. Liles Pkwy could be uniquely positioned as a place that is driven by healthy choices in exercise, food, environment, and sustainability. Community gardens, farm to table restaurants and urban mini farms could anchor key catalyst lifestyle master planned developments and tie into the history of the area.

- Install bike facilities, parks and greenways to encourage active living.
- Install a multi-use path from Coddle Creek Greenway to Frank Liske Park as recommended in the Open Space Connectivity Analysis.
- Include future expansion plans for Frank Liske Park in the future analysis of Parks & Recreation needs in the surrounding area.

Connected & Accessible Places

A key feature of connected and accessible places is a multi-modal transportation network that links the land uses along the corridor and accounts for all users in a way that minimizes conflict. For this small area plan, the ideal transportation network will provide access from the neighborhood to employment and lifestyle centers and from employment and industrial centers to major transportation routes. Connected and accessible places create an intuitive, safe network that is attractive to users of all ages and abilities. This infrastructure attracts businesses and employers because of the high quality of life and sense of place that ample connectivity can provide.

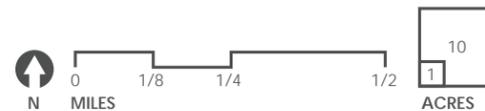
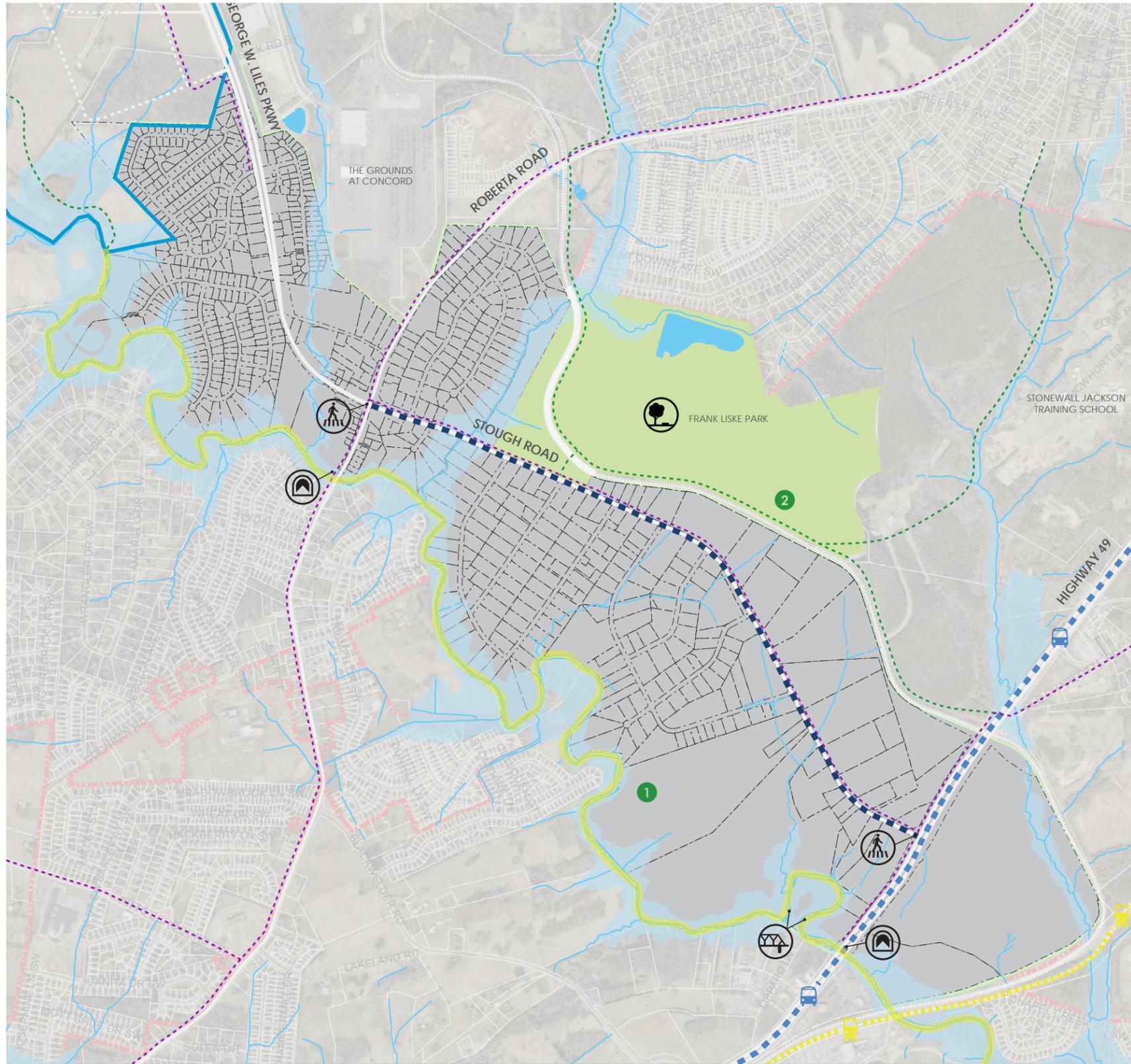
- Support NCDOT State Transportation Improvement Program (STIP) project to extend George W. Liles Pkwy all the way to Hwy 49 with a multi-use path on the east side.
- Provide safe roads for all modes of transportation includes bus, automobile, bicycle, and foot.
- Install Public Transit Bus Route connecting Concord and Harrisburg as recommended in the Long-Range Public Transportation Mater Plan.

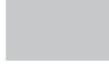
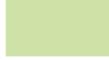
Enhanced Quality of Life

Local community culture and quality of life are both drivers for workforce development and include things like housing choices, transportation options, proximity to parks/greenways, and places for dining, shopping and entertainment based on connectivity and experience. This Small Area Plan aims to layout growth and future development along the George W. Liles Parkway in such a manner as to enhance the community's quality of life and position the George W. Liles Pkwy Corridor as a place people want to live, work and visit.

- Provide easy access from housing to workplace, dining, shopping and recreational activities via automobile, bike and/or foot.
- Ensure compatibility between neighboring land uses and provide adequate buffering between conflicting uses.
- Add more employment opportunities and industries that support The Grounds at Concord.

MAP 3-1: STUDY AREA B OVERVIEW MAP



-  Study Area
-  Green Space
-  Water
-  100 Yr Floodplain
-  Proposed Greenways
-  Proposed Multi-Use Path
-  Proposed Coddle Creek Greenway per OSCA Plan - Location TBD
-  Greenway
-  Pkwy Future Extension
-  Existing Rail Spur
-  Crosswalk
-  Park
-  Major Trailhead Parking
-  Pedestrian Tunnel
-  Pedestrian Bridge
-  Proposed Commuter Rail
-  Proposed Bus Route
-  Public Transit Stop
-  Proposed Bus Stop

3.2. EXISTING CONDITIONS

Area B comprises approximately 1,214 acres located between the Coddle Creek floodplain to the west, the Bootsmead Rail Spur to the East, Area A to the north, and parcels just south of Hwy 49. Unlike Area A, much of Area B is already developed, with a few notable exceptions towards Hwy 49.

Area B includes the established neighborhoods of Roberta Woods, Roberta Farms, Cedar Springs, Wolf Meadow Acres, Pebblebrook Acres, as well as Concord Pointe and Tower Place apartment complexes. Area B also includes Motorsports Industrial Park and a small portion of Frank Liske Park/ Stonewall Jackson Training Camp. There are isolated residential and industrial parcels along the corridor.

The largest pieces of undeveloped land are along the west side of Stough Rd., where there are about 130 acres controlled by Harris Morrison and the Stough Family, and an additional 113 acres south of the junction of Hwy 49 and Stough Rd.

Stonewall Jackson Training Camp (800 acres) is now under the ownership of Cabarrus County and is a tremendous asset adjacent to Area B. However, any future redevelopment plans for this site are unknown and therefore not accounted for in this plan.

3.2.1. EXISTING DEVELOPMENT

Of the nearly 1,214 acres located in Area B, 886.9 acres are buildable, and 347.1 acres are currently vacant. The vacant property represents just under 40% of the buildable acres.

Roberta Farms

Roberta Farms consists of approximately 161 lots, which range from about 1/4 of an acre to 1/2 an acre, zoned RM-2 Single Family, Medium Density. It is immediately adjacent to Area A. It occupies 61.1 acres for a gross density of 2.64 units per acre.

Roberta Woods

Roberta Woods is across George Liles Pkwy from Roberta Farms and is adjacent to The Grounds at Concord. It consists of about 65 lots, zoned RV-CU Residential Village, Conditional Use, with lot sizes from 1/6 of an acre to 3/4 of an acre. It occupies 22.6 acres, for a gross density of 2.87 units per acre.

There are 18 single family residential lots that line George W Liles Pkwy between Roberta Farms and Roberta Woods developments.

Concord Pointe

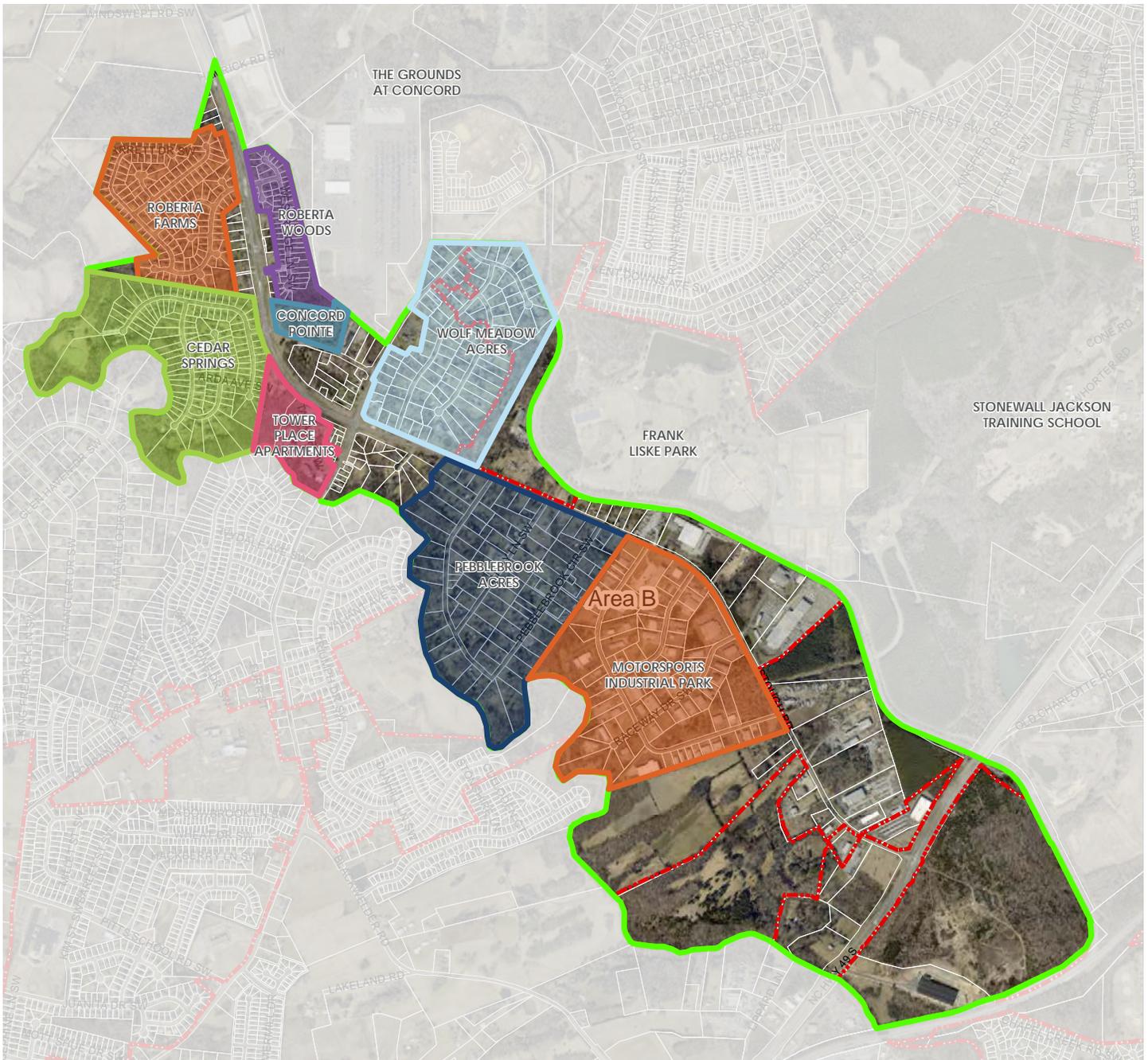
Concord Pointe apartments consists of 104 units, zoned RC Residential Compact. It is also located adjacent to the Grounds at Concord. It is located on 9.6 acres, for a gross density of 10.8 units per acre.

Cedar Springs

Cedar Springs is a single family detached neighborhood, consisting of 134 parcels. It is zoned RM-2 and lots range in size from about 1/3 of an acre to almost 9 acres. It is located on 106.6 acres for a gross density of 1-1/4 units per acre.

(THIS PAGE INTENTIONALLY LEFT BLANK)

MAP 3-2: EXISTING DEVELOPMENT MAP - 2021 AERIAL IMAGE



Tower Place Apartments

Tower Place Apartments is south of Cedar Springs approaching the intersection where George Liles Pkwy becomes Stough Rd. The apartment complex consists of 100 units on about 14.6 acres. The same ownership group owns a parcel of 7.6 acres that is adjacent on the north side of the existing development and represents the possibility of additional expansion of this complex. The parcels are zoned RC and RC-CD. The gross density of the developed portion is 6.84 units per acre.

NW Quadrant George Liles at Stough Rd. and Roberta Rd.

There are about 21.8 acres bounded by Concord Pointe Apartments, the Grounds at Concord and the intersection of George Liles and Roberta Rd. This land is zoned RM-1 and RM-2. Approximately

1/3 of this area is owned by the North Carolina Department of Transportation in preparation for future work to this section of the corridor and the remaining 14 acres has some residential and some commercial use. There is an indoor shooting range on Roberta Church Rd., but the majority of the remainder of the area is occupied by single family detached dwellings or is vacant.

Wolf Meadow Acres

Wolf Meadow Acres is another single family residential development at the northeast corner of the intersection of Stough Rd and Roberta Rd. It consists of 94 single family detached lots, about 2/3 of which are currently in the city limits of Concord and 1/3 remain in Cabarrus County on the northeastern quadrant of the intersection of Stough Rd. and Roberta Rd. The portion in the City of Concord is zoned RV. The neighborhood wraps around Roberta Baptist Church, which is zoned OI. There are 13 parcels in the same subdivision in the southeast quadrant of the intersection, which are entirely zoned RM-2. There are about 88.6 acres of the subdivision north of Stough Rd. and 13.2 acres on the south side of Stough Rd. There are also 5.5 acres in the southeast quadrant that were formerly Roberta Mills property. The mill structure would have been located in the vicinity of the Tower Place Apartments property, adjacent to Coddle Creek. NCDOT owns about 5.8 acres along Stough and Roberta Rd. from right-of-way acquisition.

Pebblebrook Acres

The final major residential development is the 94 units of Pebblebrook Acres on the southern side of Stough Rd. It is zoned RM-2 as well. It is located on 132.5 acres with a gross density of .71 units per acre.

Motorsports Industrial Park

Motorsports Industrial Park is located on about 141.9 acres and is Zoned I-1. Lot sizes range from approximately 1/2 acre to 8 acres. The industrial park is about 3/4 built out at this point, with only 24 lots remaining undeveloped of 81 total. The undeveloped land consists of approximately 40 acres.

Miscellaneous Industrial, North Side Stough Rd.

There are 55.9 acres of I-1 and I-2 properties on the north side of Stough Rd. between the entrance to Frank Liske Park, the railroad tracks, and the city limits. 5 lots, consisting of 4.13 acres are currently used for single family detached residential, two lots, consisting of 8.5 acres are currently vacant, and the remainder is developed as industrial space. The single largest industrial user, at about 20 acres, is Weyerhaeuser.

3.2.2. EXISTING INFRASTRUCTURE

Water

The existing Water System in Study Area B is much more extensive than Area A given the increase of developed vs. undeveloped land. The existing distribution main water lines run along George W. Liles Parkway (24 inch), Stough Road (8 inch), and Roberta Road (8 inch). The existing water lines also extend into the areas of existing development within Study Area B including those mentioned in the previous section, Roberta Farms, Roberta Woods, Concord Pointe, Cedar Springs, Wolf Meadow Acres, Pebblebrook Acres, and Motorsports Industrial Park.

Sewer

Sewer is development driven and existing sewer in Area B, like the water system, is more extensive than in Area A given development in the area.

Any proposed future development in this area would need additional Sewer added. As development occurs developers will be required to work with the City to assess sewer needs and capacity. Sewer capacity discussions will involve The Water & Sewer Authority of Cabarrus County (WSACC) as well. The amount of sewer available for allocation and attributable by any designated calendar year is given by WSACC as determined through the interlocal agreement and Concord's City Council. The City's Sewer Allocation Policy can be viewed on the City's website at: <https://concordnc.gov/Departments/Engineering/Development-Standards>

Stormwater

More existing development in Study Area B vs Study Area A comes with more hardscape. Therefore instead of stormwater being routed through ditches like in Area A, there is a combination of stormwater ditches and pipes in Area B.

As development continues within this area, protection of the floodplain is still critical. Unmanaged upstream urbanization of a floodplain results in a dramatic increase in the flood storage needs of the creeks as they traverse Concord. With an increase in the width of the floodplain comes a greater incidence of flooding and a reduction in overall amount of property available for development.

Transportation

George W Liles Pkwy – The main North-South corridor along Study Area B is George W. Liles Pkwy. It is a four-lane divided highway up until about the entrance of the Cedar Springs Neighborhood where it transitions to a two-lane highway until it reaches the intersection with Roberta Road and turns into Stough Road. No recorded vehicles per day (vdp) are available on the NCDOT 2020 average daily traffic map for the Parkway between Weddington and Stough Road.

Roberta Road – Roberta Road is a two-lane roadway classified as a minor thoroughfare that collects traffic from adjoining neighborhoods. The roadway has ultimate connections to Old Charlotte Road to the north and Hwy 49 to the south. The average daily traffic volumes were recorded as 6,800 vpd near the intersection of George W. Liles Pkwy/Stough Road.

Stough Road – Stough Road is a two-lane roadway that starts where George W. Liles Pkwy ends at the Roberta Road intersection and traverses south, terminating at Hwy 49. This road provides access to Frank Liske Park, Motorsports Industrial Park and various residential neighborhoods. The average daily traffic volumes were recorded as 9,100 vpd.

Hwy 49 – Hwy 49 in the vicinity of Study Area A exists as a four-lane divided highway with a posted speed limit of 55 mph. The highway generally parallels Concord Parkway (Hwy 29) and provides connections to Charlotte in the south and Mount Pleasant in the north. The NCDOT average daily traffic count map indicates a recorded volume of 28,000 vpd on Hwy 49 in the vicinity of Study Area B.

Public Transit

Currently no existing public transit occurs in or around Study Area B.

Parks & Recreation

Coddle Creek – Coddle Creek, a meandering stream and floodplain along the southwestern boundary of the study area has banks as wide as 12-16 feet in places. Further upstream near Poplar Tent Road, it has been incorporated into Dorton Park, a 24 acre park with tennis courts, playground equipment, soccer fields, frisbee golf, a 1.1 mile gravel walking trail, picnic shelters, restrooms and a concession stand. Dorton Park is located in Afton Village, a traditional neighborhood between Poplar Tent Road and George W. Liles Parkway.

The 100 year floodplain surrounding Coddle Creek is quite significant even though its headwaters are impounded as Lake Howell. Coddle Creek travels southeast past Frank Liske Park and eventually ties into the Rocky River.

Frank Liske Park – County owned, Frank Liske Park sits on 238 acres mostly outside of the Study Area B boundary. Park amenities include four separate rental areas, a 10-acre lake for fishing and paddle boating, picnic shelters, picnic sites, horseshoe pits, volleyball courts, playgrounds and six lighted tennis courts. Additional amenities include a 11-field soccer complex with two lighted fields, a four-field lighted softball complex, walking and fitness trails, a nature trail and an 18-hole miniature golf course. The soccer complex is operated in cooperation with Charlotte Soccer Academy and the City of Concord Parks and Recreation Department.



Frank Liske Park | Jackson Training School Lake



City of Concord | Fire Station No. 12

Fire Department

This area of Concord is currently served by Fire Station No. 5 and newly opened Fire Station No. 12.

Fire Station No. 5 – Dedicated in 1991, Concord Fire Station No. 5 is located at 170 Pitts School Road. Originally operated by the Pitts Volunteer Fire Department, the structure was renovated and expanded in 1991 as a result of an agreement between the Pitts VFD Board of Directors and the City of Concord for the Pitts Fire District to be protected by the Concord Department of Fire & Life Safety, and that all equipment, vehicles, and structures owned by the Pitts Fire District be given to the City of Concord. Thus, since 1991, Concord Engine Company 5 has been responsible for this area. Engine Company 5, and a reserve engine is quartered in this station.

Fire Station No. 5's first due area includes the Charlotte Motor Speedway, a mixture of residential, multi-family, industrial, commercial occupancies as well as the Hwy. 29 and Speedway Boulevard corridors.

Fire Station No. 12 – Newly opened Fire Station No. 12 is located at 3300 Roberta Road.

Environmental

Map 3.4 was created using the USDA Soil Data Explorer tool and applies to dwellings with or without a basement and small commercial structures. The soil map includes the map for the defined area of interest, a list of soil map units on the map and extent of each map unit, and cartographic symbols displayed on the map. Below is a description of the various soil types found in this area.

Soil Types

MeB: Mecklenburg loam, 2 to 8 percent slopes (593.7 acres, 47.9%)

IdA: Iredell loam, 0 to 2 percent slopes (267.1 acres, 21.5%)

ChA: Chewacla sandy loam, 0 to 2 percent sloped, frequently flooded (113.4 acres, 9.1%)

Remaining 21.5%:

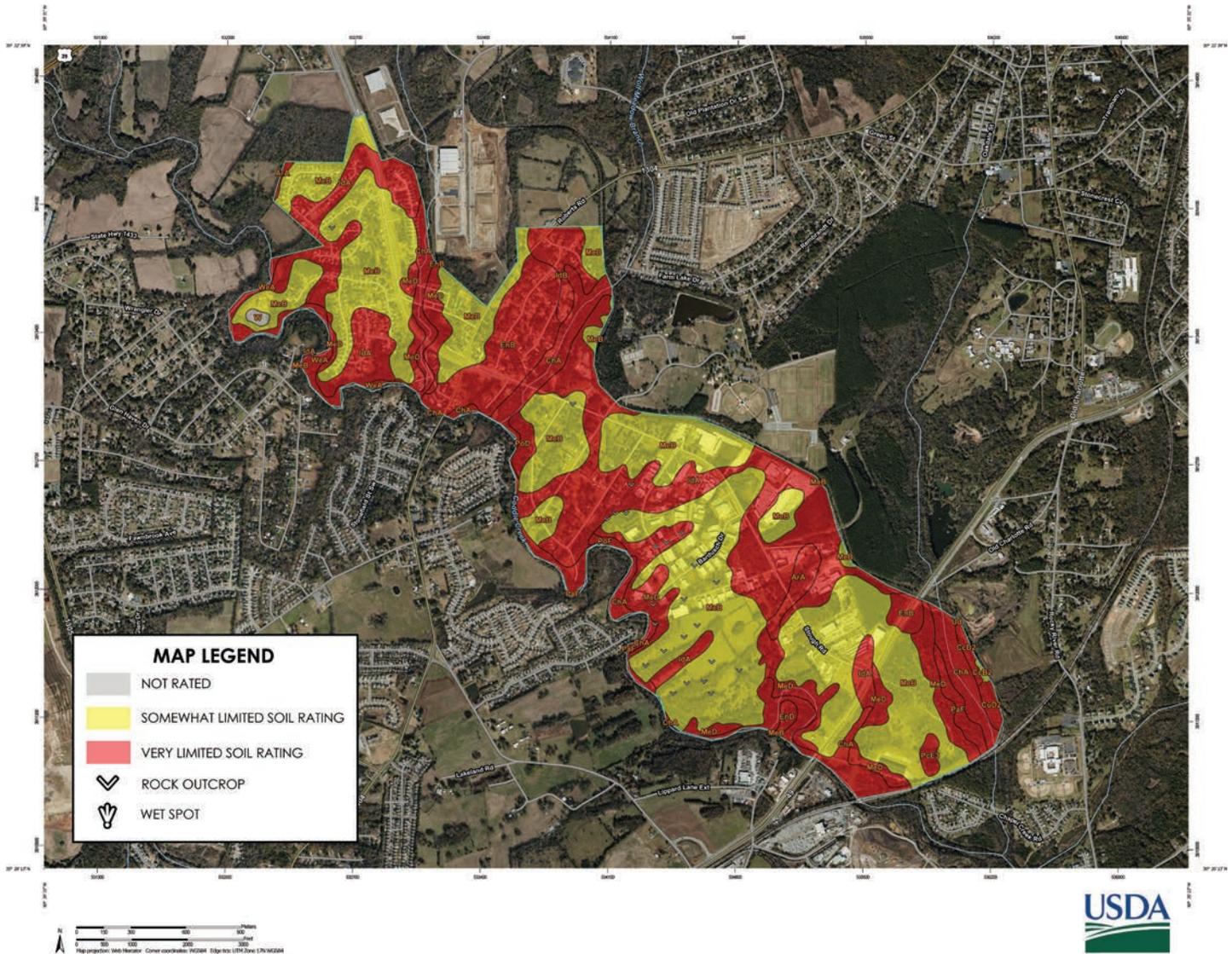
- MeD: Mecklenburg loam, 8 to 15 percent slopes (97.9 acres, 7.9%)
- EnB: Enon sandy loam, 2 to 8 percent slopes (77.9 acres, 6.3%)
- IdB: Iredell loam, 2 to 6 percent slopes (21.7 acres, 1.7%)
- WeA: Wehadkee loam, 0 to 2 percent slopes, frequently flooded (16.3 acres, 1.3%)
- ArA: Armenia loam, 0 to 2 percent slopes, rarely flooded (13.3 acres, 1.1%)

Less than 1% each:

- PaF: Pacolet sandy loam, 15 to 35 percent slopes (10.0 acres, 0.8%)
- PoD: Poindexter loam, 8 to 15 percent slopes (7.0 acres, 0.6%)
- PcE3: Pacolet-Udorthents complex, 12 to 25 percent slopes, gullied (4.6 acres, 0.4%)
- CuD2: Cullen clay loam, 8 to 15 percent slopes, moderately eroded (51. Acres, 0.4%)
- CcD2: Cecil sandy clay loam, 8 to 15 percent slopes, moderately eroded (3.8 acres, 0.3%)
- PoF: Poindexter loam, 15 to 45 percent slopes (2.1 acres, 0.2%)
- W: Water (2.2 acres, 0.2%)
- CcB2: Cecil sand clay loam, 2 to 8 percent slopes, moderately eroded (0.9 acres, 0.1%)

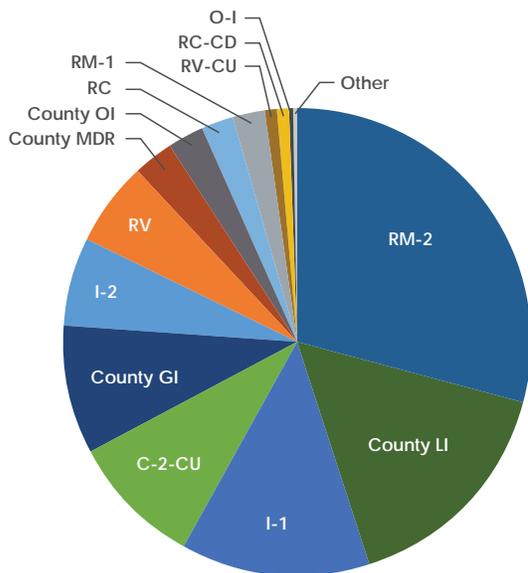
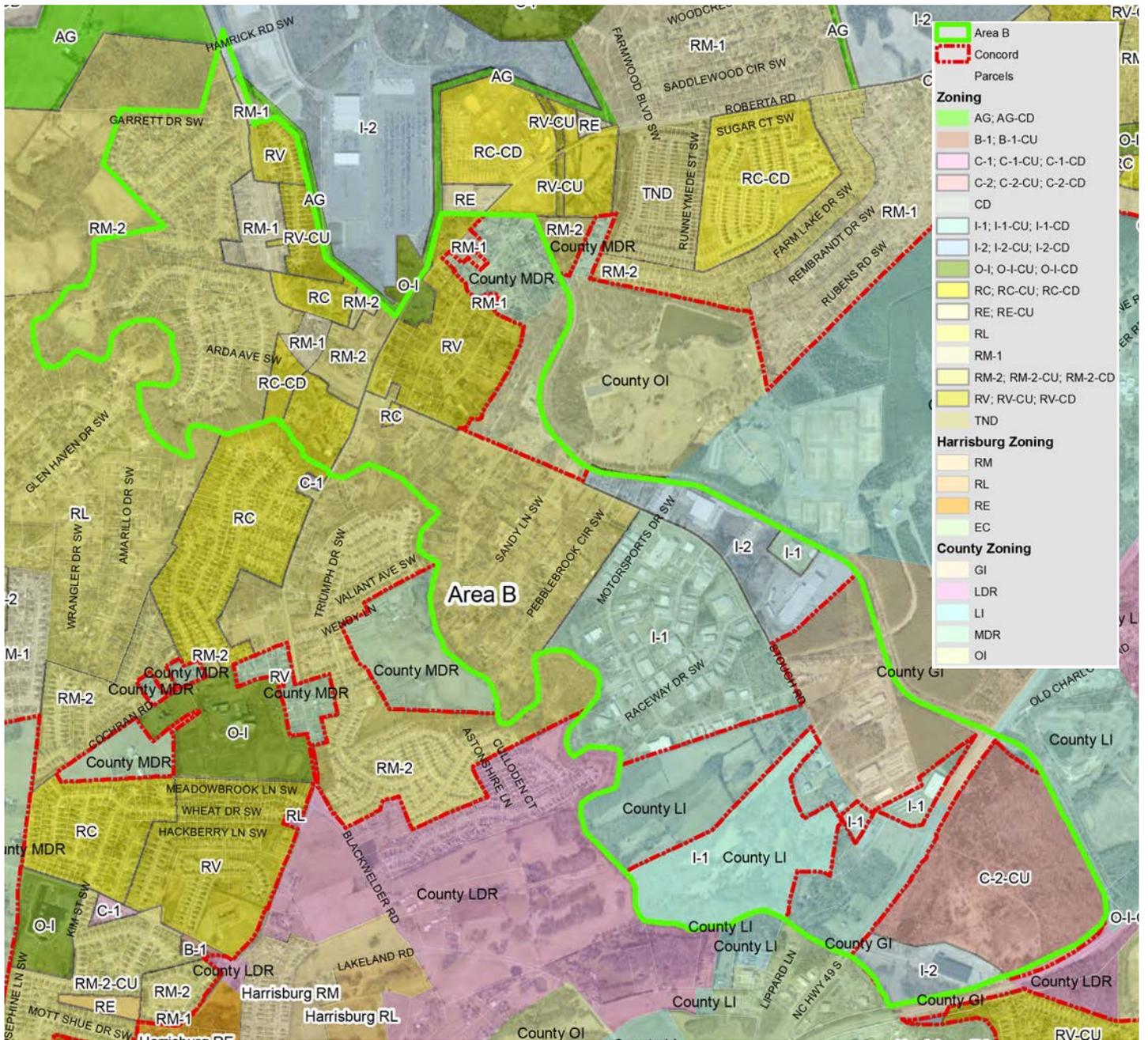
The soil types present in this area are consistent with of those in the surrounding properties. It should be noted that this is an assessment of the existing conditions and a “somewhat limited” or “very limited” soil rating is not an indication that development cannot occur in an area but that the soil may require amendments and modification.

MAP 3.4: USDA SOIL DATA EXPLORER MAP - DWELLINGS & SMALL COMMERCIAL STRUCTURES



Source: <https://websoilsurvey.sc.egov.usda.gov/App/WebSoilSurvey.aspx>

MAP 3-5: AREA B - EXISTING ZONING MAP



AREA B PERCENT LAND AREA BY ZONING

ZONING	ACRES	%
C-2-CU	114.1	9.3%
I-1	159.5	13.0%
I-2	77.5	6.3%
O-I	3.9	0.3%
RC	29.4	2.4%
RC-CD	7.5	0.6%
RM-1	24.6	2.0%
RM-2	360.3	29.3%
RV	70.7	5.7%
RV-CU	13.6	1.1%
County GI	108.3	8.8%
County LI	194.8	15.8%
County MDR	32.6	2.6%
County OI	31.5	2.6%
Other	2.3	0.0%
TOTAL	1,230.7	100%

3.2.3. EXISTING ZONING

Zoning establishes a list of uses that are permissible on a particular parcel, which is driven by the Concord Development Ordinance. While particular uses are subject to change over time, the general purpose of each zoning district is relatively stable.

Over 100 Acre Zoning Districts

RM-2 – Residential Medium Density: Medium density single family; 4 dwelling units per acre (dua) max., where adequate public facilities exist. 360.3 acres, Area B.

County LI – Limited Industrial: This district provides for both large and small scale industrial and office development. The primary distinguishing feature of this district is that it is geared to indoor industrial activities which do not generate high levels of noise, soot, odors or other potential nuisances/pollutants for impacting adjoining properties. 194.8 acres, Area B.

I-1 – Light Industrial: Mix of light manufacturing uses, office park and limited retail and services in business park setting. Should have direct access to thoroughfare. 159.5 acres, Area B.

C-2 – General Commercial: General commercial activities; located on or near major thoroughfares. 114.1 acres, Area B.

County GI – General Industrial: While this district permits both large and small scale industrial and office development, its primary purpose is to provide a location for large scale development. It is designed to permit a very wide variety of industrial uses which may occur both indoor and outdoor, including certain land uses which are permitted in no other zoning district because of their potential to create nuisances for adjoining properties. 108.3 acres, Area B.

Under 100 Acre Zoning Districts

I-2 – Heavy Industrial: Areas of heavy and concentrated fabrication, manufacturing, and industrial uses which are suitable based upon adjacent land uses, access to transportation, and the availability of public services and facilities. Should have direct access to thoroughfare. 77.5 acres, Area B.

RV – Residential Village District: Detached and attached single family with max. of 8 dwelling units per acre. 84.3 acres, Area B.

County MDR – Medium Density Residential: This district is intended to permit development with a moderately high density community character including a variety of housing types, including townhouses. This district is located where public utilities are available. 32.6 acres, Area B.

County OI – Office Institutional: This district is intended to accommodate relatively low intensity office and institutional uses at intensities complementary to residential land use. 31.5 acres, Area B.

RC – Residential Compact: High density residential including single family attached and multi-family, up to 15 dwelling units per acre. 36.9 acres, Area B.

RM-1 – Residential Medium Density: Medium density single family; 3 dwelling units per acre max., where adequate public facilities exist. 24.6 acres, Area B.

Under 10 Acre Zoning Districts

O-I – Office-Institutional: Agencies and offices rendering specialized services and traditional institutional functions (both public and private). 3.9 acres, Area B.

AG – Agricultural District: Agricultural preservation areas. 0.9 acres, Area B.

County GC – General Commercial: The primary purpose of this district is to provide locations for large scale commercial activities. This level of commercial activity usually draws clientele regionally as well as from nearby neighborhoods, requires siting on major thoroughfares, and requires relatively large-scale off street parking areas. 0.7 acres, Area B.

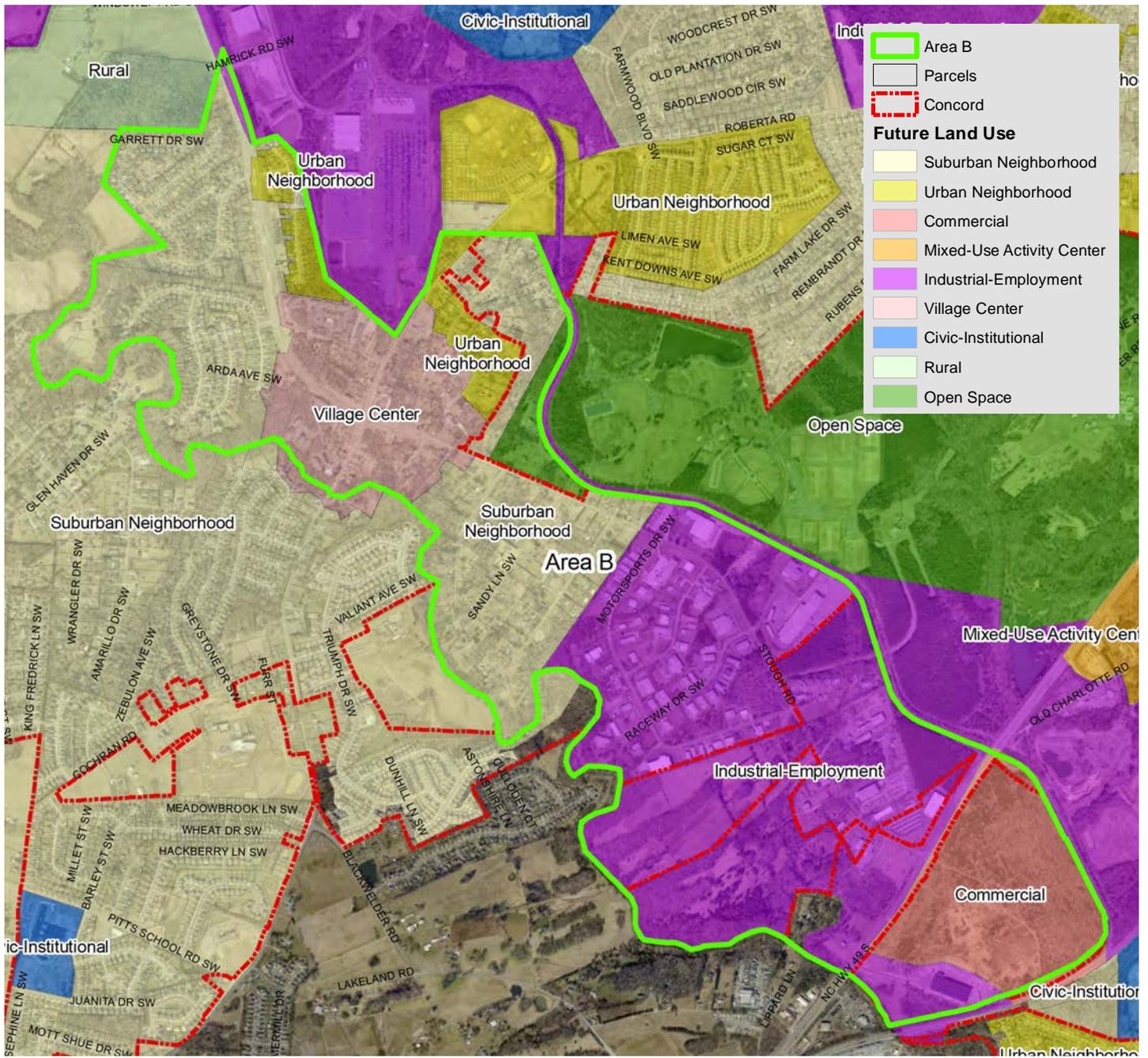
County LDR – Low Density Residential: This district is intended to permit development with a low density residential community character. This district is located where public utilities are available or are envisioned to be available within the next two to five years. 0.6 acres, Area B.

-CU indicates that a site has a Conditional Use Zoning.

-CD indicates that the site is a Conditional District.

(THIS PAGE INTENTIONALLY LEFT BLANK)

MAP 3-6: AREA B - FUTURE LAND USE MAP

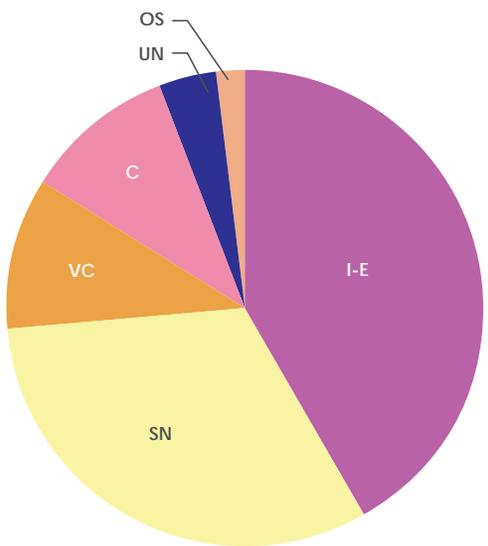


Legend

- Area B (Green outline)
- Parcels (White outline)
- Concord (Red dashed outline)

Future Land Use

- Suburban Neighborhood (Light Yellow)
- Urban Neighborhood (Yellow)
- Commercial (Pink)
- Mixed-Use Activity Center (Orange)
- Industrial-Employment (Purple)
- Village Center (Light Pink)
- Civic-Institutional (Blue)
- Rural (Light Green)
- Open Space (Green)



AREA B - FUTURE LAND USE CATEGORIES

CATEGORY	ACRES	%
INDUSTRIAL-EMPLOYMENT	504.5	41.9%
SUBURBAN NEIGHBORHOOD	383.7	31.9%
VILLAGE CENTER	125.0	10.4%
COMMERCIAL	121.2	10.1%
URBAN NEIGHBORHOOD	48.9	4.1%
OPEN SPACE	20.9	1.7%
TOTAL	1,204.2	100%

3.2.4. 2030 LAND USE PLAN

Future Land Use Categories

The entire corridor of study encompasses almost every future land use category in the 2030 Land use Plan.

Industrial-Employment – In the southern area of the small area plan, the majority of land is designated as Industrial Employment: 7,504.5 acres, or 41.9% of the land area of Area B:

The intent of the Industrial/Employment (IE) Future Land Use category is to identify those areas that have either already developed as industrial or are suited for additional industrial development due to the presence of infrastructure and access to transportation routes, such as major highways and railroads. These industrial areas should be preserved for employment uses to generate jobs for the community.

Industrial uses have already developed in several areas throughout the community, most of which are light industrial in function and impact. The 2030 Future Land Use Plan identifies the need to protect industrial lands, and to encourage additional growth in industrial/employment uses in designated areas, particularly around Concord Regional Airport, and along I-85 between Pitts School Road and Rocky River.

Additionally, some light industrial/employment uses are encouraged to locate in Mixed-Use Districts, depending upon their intensity, as identified earlier in this section.

Suburban Neighborhood – The second largest land area in Area B of the Small Area Plan is designated Suburban Neighborhood: 383.7 acres, or 31.9%.

The Suburban Neighborhood Future Land Use category includes single-family areas that are formed as subdivisions or communities, with a relatively uniform housing type and density throughout. They may support a variety of single-family detached residential types, from low-density single-family homes to denser formats of smaller single-family homes. In areas designated Suburban Neighborhood, homes are typically buffered from surrounding development by transitional uses or landscaped areas. Single-family attached dwellings are an option for infill development with the suburban neighborhood future land use category. These neighborhoods are often found in close proximity to suburban commercial, office, and industrial centers, and help provide the consumers and employment base needed to support these centers. Lots at intersections of collector and arterial streets within or at the edges of suburban neighborhoods may support small-scale, neighborhood serving, pedestrian-oriented commercial or service uses such as coffee shops, cafes, beauty salons and light retail. Pedestrian oriented uses exclude uses with drive-in, drive-through or automobile related services.

Village Center – Both the Village Center and Commercial Future Land Use categories have a substantial presence within the small area plan limits. Village Center has 125.0 acres, or 10.4% and Commercial has 121.2 acres, or 10.1% of Area B.

Village Center areas are typically located at key intersections of minor transportation corridors. These centers are often located in areas with Future Land Use categories that are residential and/or rural

in focus with limited non-residential development nearby. Oftentimes areas designated as Village Centers already contain some non-residential development, usually highway oriented retail, office, or light industrial, with supportive zoning. Village Centers represent opportunities to create small to mid-size commercial and mixed-use centers and limit strip commercial in areas that are predominantly residential in character.

As with the larger Mixed-Use Activity Centers, the smaller Village Center creates an alternative to strip commercial development by concentrating non-residential uses in areas where infrastructure and residential demand support them. Consequently, none of the designated Village Centers proposed would develop without residential growth preceding it, thus creating the demand. Similarly, Village Centers would not develop if there was inadequate infrastructure, such as roads, water and sewer service.

The characteristics of developed Village Centers will vary greatly depending on the surrounding land use pattern and existing development intensity. The types of uses within Village Centers will vary, but encouragement should be given to mixed uses and those uses that are neighborhood serving (i.e. restaurants/diners, gas stations, small convenience stores, hardware stores, etc.) which reduce the need for residents to travel long distances in a car to meet basic needs. Additionally, emphasis should be given to pedestrian oriented development and interconnectivity between uses within the Village Center and out to the surrounding residential areas.

Commercial – The Commercial Future Land Use category includes a mix of commercial land use types. While these areas continue to support additional commercial development and redevelopment, much of the new commercial development should be concentrated within the Mixed-Use activity Centers and Village Centers. Strip commercial development along major corridors is discouraged in the 2030 Future Land Use Plan. Instead commercial development integrated into Mixed-Use activity Center at key intersections is desired. Areas designated as Commercial are intended to represent those that include a variety of commercial uses at different intensities, including large scale malls, lifestyle centers, and community shopping centers.

Other – There are limited amounts, 48.9 acres, or 4.1% designated as Urban Neighborhood and 20.9 acres, or 1.7% designated as Open Space in Area B.

3.3. FUTURE FOCUS

3.3.1. LAND USE & BUILT FORM

Lifestyle Focus

Area B is almost completely built out. Respondents indicated that they want to see more Recreation, Dining, and Workplace/Employment in Area B. Most of the recreation needs for Area B should be accommodated by Frank Liske Park, which is immediately adjacent to the study area. The primary enhancement would be to the pedestrian/bicycle access along Coddle Creek and George Liles Pkwy/Stough Rd to provide access to the park. The State of North Carolina's recent sale of the park property and adjacent land to Cabarrus County creates opportunities to expand the park's existing facilities.

There are also two additional opportunities within Area B for a future lifestyle focused development: one is at the intersection of Stough Rd/George Liles Pkwy and Roberta Church Rd. This area is designated as a Village Center and development of a lifestyle focus at this intersection would involve significant redevelopment of existing properties.



Springfield Town Center | Fort Mill, SC

The other principle lifestyle focus area is at the intersection of Stough Rd. and Hwy 49. There are some access challenges for this site due to traffic patterns on Hwy 49 that prohibit direct entry or exit from Stough Rd, and limit the site to right-in, right-out access from Hwy 49. The site consists of 113 acres with a future land use designation of Commercial and conditional zoning currently in effect for a 765,000 square feet commercial development. Developers have been interested in rezoning the site to decrease commercial square footage and increase residential units. Staff is unclear this would be consistent with the community vision for the site. See further discussion under workplace focus.

Workplace Focus

Staff anticipates a significant amount of employment opportunities for the general area with the continuing subdivision of the Grounds at Concord property to the east side of George Liles Pkwy for industrial uses. The principal Grounds at Concord site is outside the study area, but has a significant impact on Areas A and B. In addition to direct employment stemming from the development of the Grounds property, staff also expects secondary employment will develop in Area B to complement the employment at the Grounds.

Key points for future workplace focus will be monitoring connectivity and access along the peripheral road system. There are existing industrial support areas in Area B at Motorsports Industrial Park and industrial property developed in Cabarrus County and the City of Concord on the east side of Stough Rd. Industrial support areas may expand through the continued build out of Motorsports Industrial Park, redevelopment of some of the properties on the east side of Stough Rd., and through the development of approximately 140 acres of currently undeveloped property adjacent to Motorsports Industrial Park and Hwy 49.



Motorsports Industrial Park | Concord, NC

City staff recommends considering the adoption of modified industrial standards for industrial zoned property within Area B that is consistent with the standards currently implemented in the Motorsports Industrial Park. See Appendix B: Area B - Industrial Overlay District for the recommended standards.

The Morrison Ridge parcel is located where Stough Rd. intersects Hwy 49. This parcel is currently entitled for a large commercial development, but if desired, changing the 2030 Land Use Plan designation from Commercial to Industrial/Employment would be more consistent with much of the development in its vicinity.

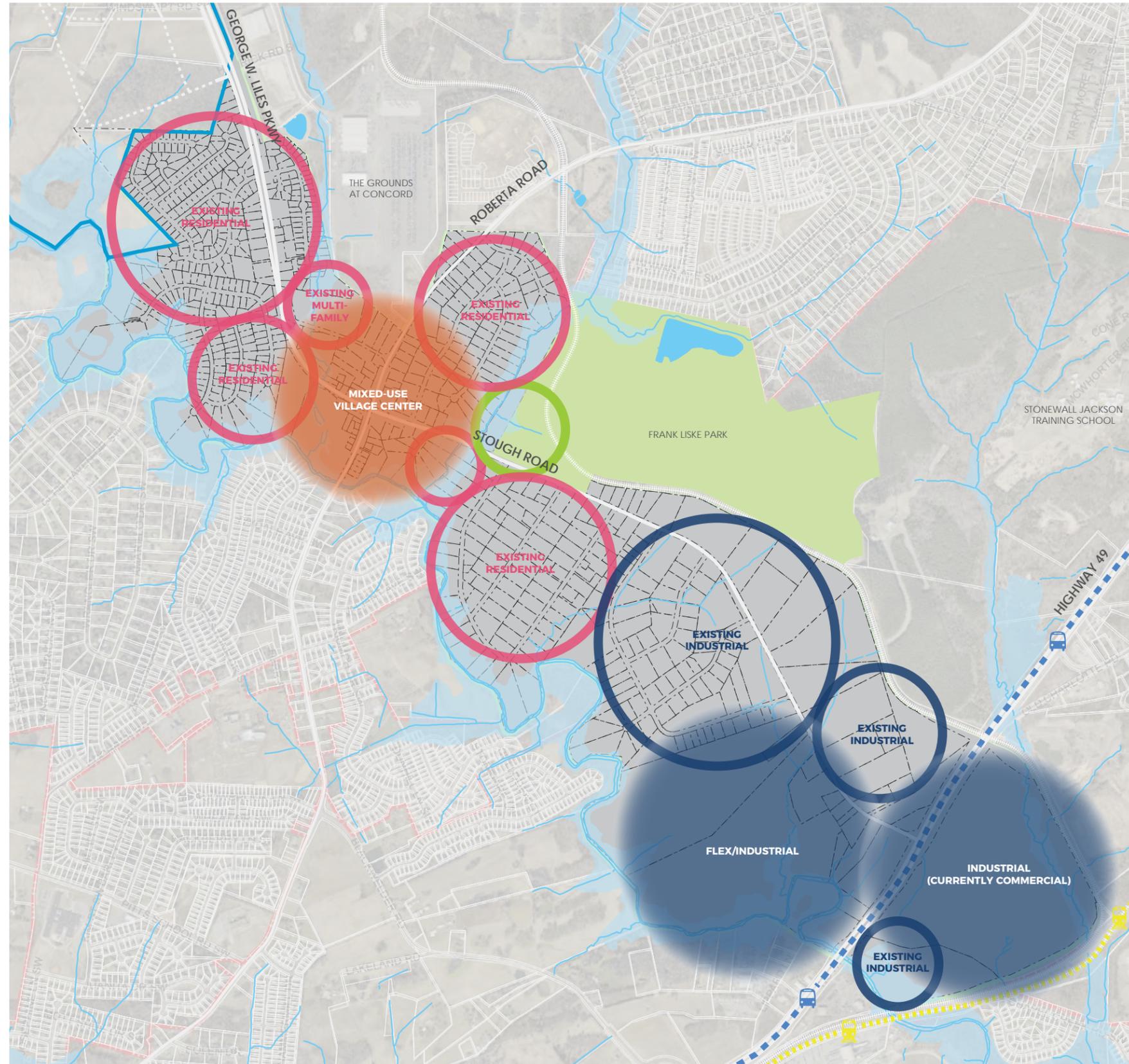
Residential

Staff anticipates residential expansion within Area B to mostly take the form of infill development, with the exception of the potential lifestyle center mentioned in the Area B Lifestyle Focus section. Staff does not anticipate any new significant residential development.

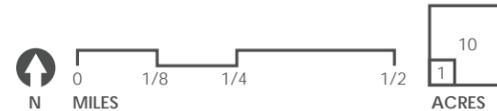
Open Space

Area B is largely developed with the exception of some of the larger vacant parcels covered in other sections of the Area B Future Focus. However, the immediately adjacent Frank Liske Park and Stonewall Jackson Training Camp site now under the ownership of Cabarrus County is a tremendous open space asset within and adjacent to Area B. Additionally, there is floodplain along Coddle Creek that will also be preserved as open space.

MAP 3.7: AREA B - LAND USE BUBBLE DIAGRAM



-  Existing Mixed-Use
-  Proposed Mixed-Use
-  Existing Industrial
-  Proposed Industrial
-  Existing Residential
-  Existing Green Space



3.3.2. TRANSPORTATION

The following transportation recommendations are a result of information gathered through meetings with the Transportation Department, NCDOT, key property owners and comments gathered from public input.

George W. Liles Pkwy Extension to Hwy 49

The road widening of the existing Stough Road for the extension of George W. Liles Pkwy from Roberta Road to Hwy 49 is in the NCDOT 2020-2029 State Transportation Improvement Program (STIP). The STIP is a multi-year capital improvement document which denotes the scheduling and funding of construction projects across the state over a minimum four year time period as required by Federal law. NCDOT's STIP covers a 10 year period, with the first six years referred to as the delivery STIP and the latter four years as the developmental STIP.

At the time of this writing, according to the STIP this project is scheduled for Right of Way (ROW) in 2029 with construction to follow after ROW. They do not have a year listed for construction and refer to it as "post year". The continued widening and extension of George W. Liles Pkwy will greatly impact Area B in the look, feel and mobility in this area.



(THIS PAGE INTENTIONALLY LEFT BLANK)

North U-Turn on Hwy 49

One of the current needs on Hwy 49 north of the Stough Road intersection is a U-Turn Area. Drivers currently must navigate towards Old Charlotte Road before they have an opportunity to make a U-Turn. The aerial maps of this area show that vehicles have made their own turn around areas to avoid the inconvenience. As the property southeast of Hwy 49 is developed this U-Turn will be critical for the mobility of the surrounding area.



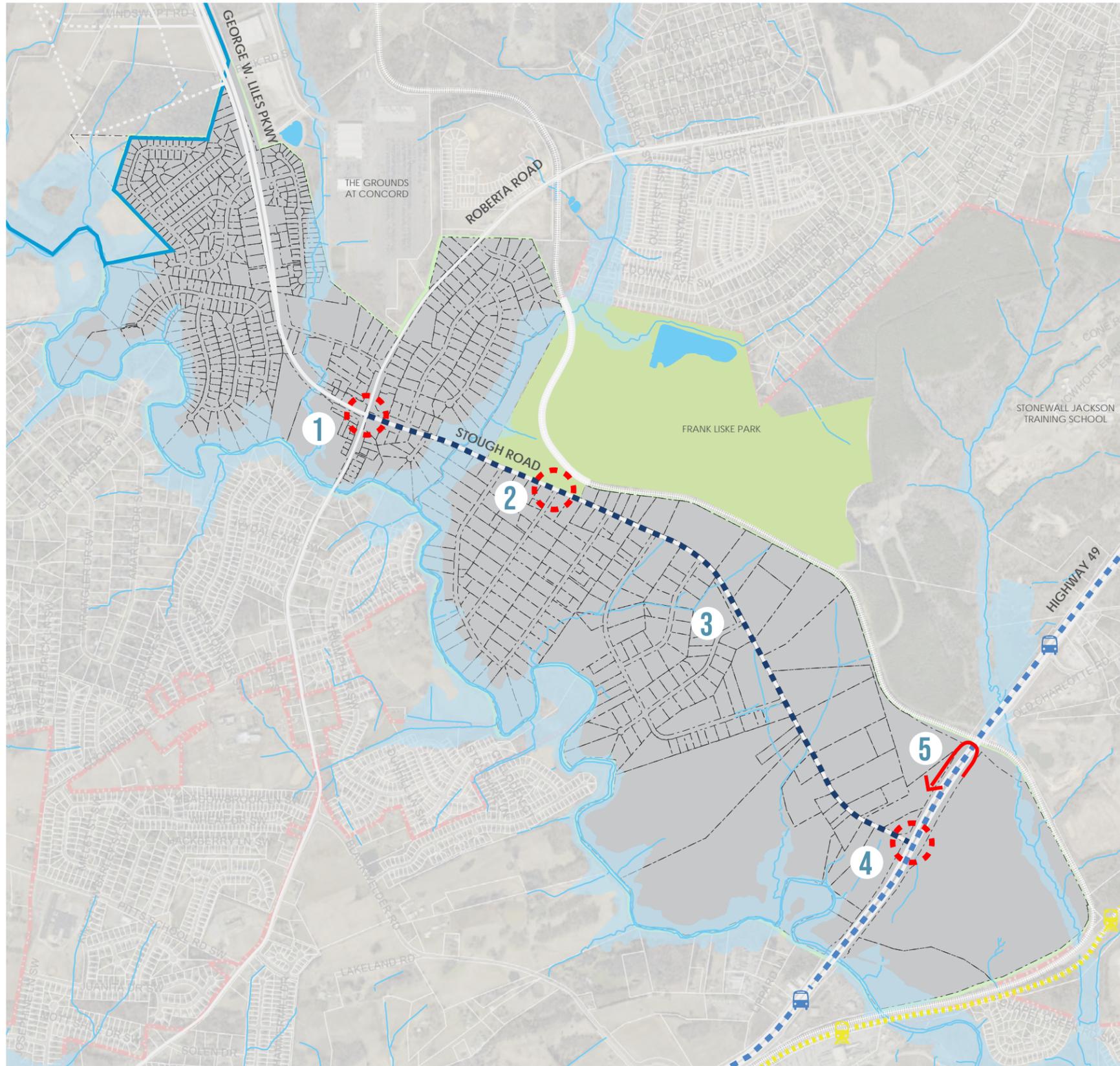
Stough Road & Highway 49 Intersection



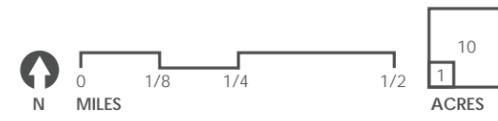
George W. Liles Parkway & Roberta Road Intersection

STUDY AREA B | FUTURE FOCUS

MAP 3.8: AREA B - TRANSPORTATION & PUBLIC TRANSIT RECOMMENDATIONS



- 1 Intersection Improvements
- 2 Intersection Improvements
- 3 George W. Liles Pkwy Extension to Hwy 49
- 4 Right In Right Out Only
- 5 Hwy 49 U-Turn Area
- Green Space
- Water
- 100 Yr Floodplain
- Pkwy Future Extension
- Significant Intersections
- Existing Rail Spur
- Proposed Commuter Rail
- Proposed Bus Route



Intersection Improvements at Roberta Road

The Open Space Connectivity Analysis Plan identifies the intersection of the George W. Liles Parkway and Roberta Road as a “need improvement” intersection.

Currently there are no sidewalks leading to this intersection and as a result no pedestrian friendly crossings. As sidewalks and multi-use paths are added in this area improvements will be needed to provide safe crossing for modes of travel other than vehicles.

Entry Improvements at Frank Liske Park

There are no current plans in place, however, with Frank Liske Park and Stonewall Jackson Training Camp now under the ownership of Cabarrus County this area could see growth of the existing park. Also, the widening of Stough Road and extension of George W. Liles Pkwy to Hwy 49 will require changes to the existing entry of Frank Liske Park.



Stough Road & Frank Liske Park Entrance

This plan identifies the entrance to Frank Liske Park as a significant intersection on the Corridor that requires full movement in order for vehicles to turn both right and left out of and into the Park. Another request is to find a second entrance for the Park along the George W. Liles Pkwy extension. The location for this second entrance hasn't been identified although Barousch Road would require the least amount of interventions along the corridor and rail spur.

(THIS PAGE INTENTIONALLY LEFT BLANK)

(THIS PAGE INTENTIONALLY LEFT BLANK)

3.3.3. PUBLIC TRANSIT

The following section summarizes the Long Range Public Transit Plan (LRTP) as it pertains to the George W. Liles Parkway Study Area B. For more information on the 2020 Cabarrus County Long Range Public Transit Plan see Page 69.

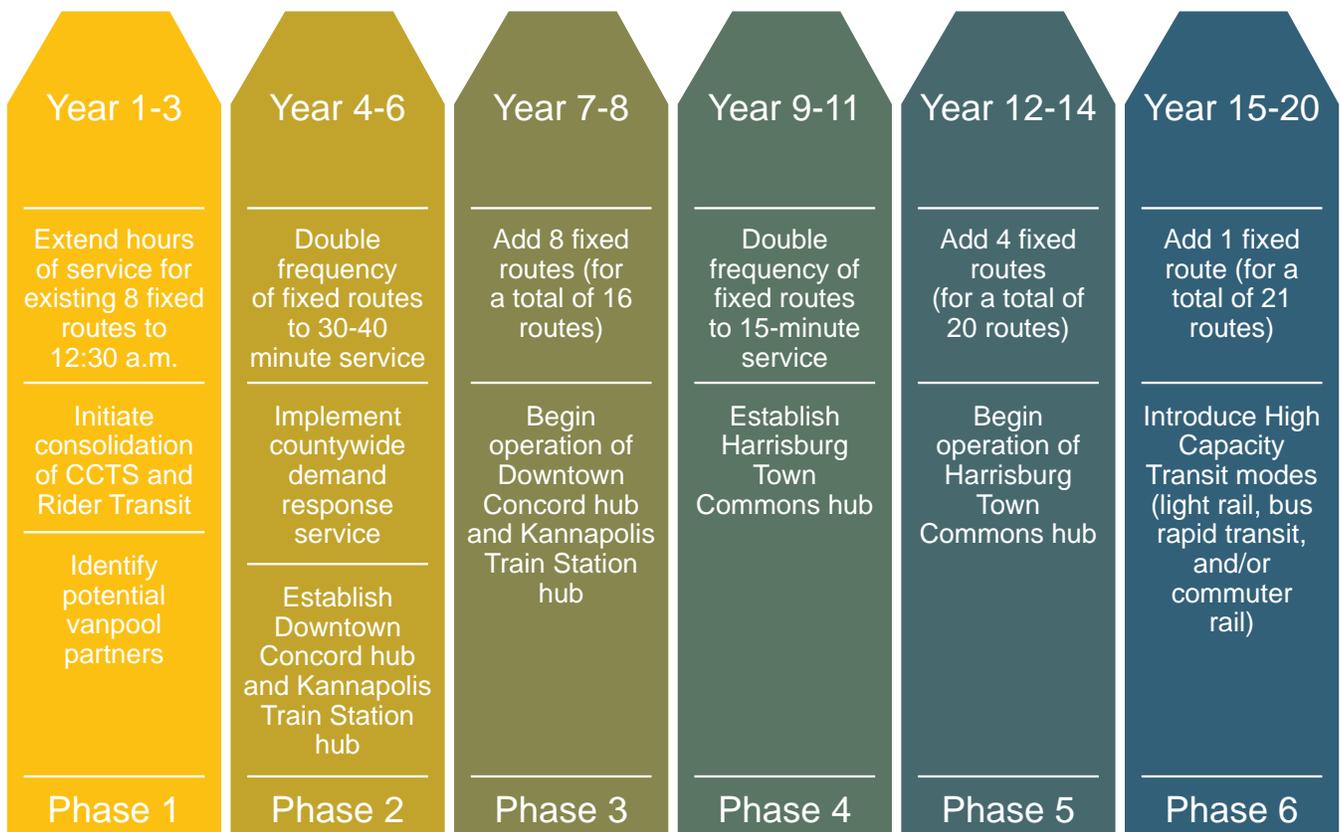
2020 Cabarrus County Long Range Public Transit Plan

Cabarrus County Transportation Service (CCTS) and Concord Kannapolis Area Transportation (Rider) initiated a Long Range Public Transportation Master Plan with the intent of creating a strong vision for improved public transportation throughout Cabarrus County to address needs not only today, but far into the future.

Phase 5 Service Recommendations (Year 12-14)

Four new routes are recommended in Phase 5 that would expand Fixed Route service in Concord and Kannapolis, and extend service into Harrisburg and Huntersville. The Harrisburg Town Center hub would open in Phase 5 in order to support Harrisburg Fixed Route service. The CCX frequency would increase from 30 minutes to 15 minutes on Saturday and Sunday to more closely match the LYNX Blue Line operating schedule. Four vehicles would be required to operate the CCX. Overall Fixed Route

FIGURE 3-1: 20-YEAR PLAN - PHASED SERVICE RECOMMENDATIONS





Saturday and Sunday frequency would increase from 60 minutes to 30 minutes on all routes except Route 101, Concord Mills Circulator, CCX (all at 15 minute frequency), and the Charlotte Huntersville Express (45 minute frequency), which all would match their weekday frequency.

Route 304 – Connections with Harrisburg

According to the Cabarrus County Long Range Public Transit Plan, the Town of Harrisburg is not currently served by Fixed Route service and is often mentioned in requests for service. Current Longitudinal Employer–Household Dynamics (LEHD) data reveal that the majority of Harrisburg residents commute to locations outside the Harrisburg community. Further analysis shows that both Charlotte and Concord are the top two places where Harrisburg residents commute to and from.

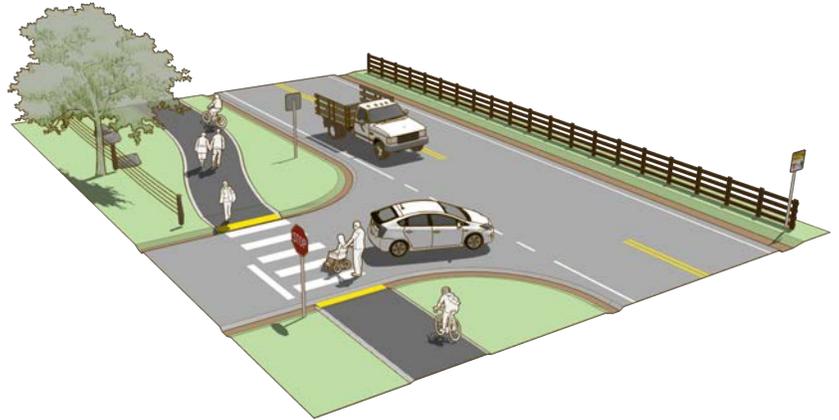
Route 304 is recommended in Phase 5 connecting Harrisburg with Concord and Charlotte. Route 304 would depart from the Downtown Concord transit hub, taking Union Street south across Highway 49 to the Southgate Commons Shopping Center on US Highway 601. Union Street is another corridor that does not have Fixed Route transit service today, but Rider Transit has received requests for providing some level of Fixed Route service since the inception of service in 2004. Service on Union Street was also requested through the LRTP public engagement process. Southgate Commons Shopping Center could be identified as a park and ride lot for commuters. The route would then continue down Highway 49, stopping at the Harrisburg transit hub at Town Center. After Town Center, Route 304 would continue on NC 49 and arrive at the University City LYNX Station.

For a map of Route 304 please see Appendix D.

3.3.4. PARKS & RECREATION

Parks & Recreation - Open Space Connectivity Analysis Plan

The following section summarizes the Open Space Connectivity Analysis Plan as it pertains to the George W. Liles Parkway Small Area Plan Study Area B. For more information on the 2019 Open Space Connectivity Analysis Plan see Page 74.



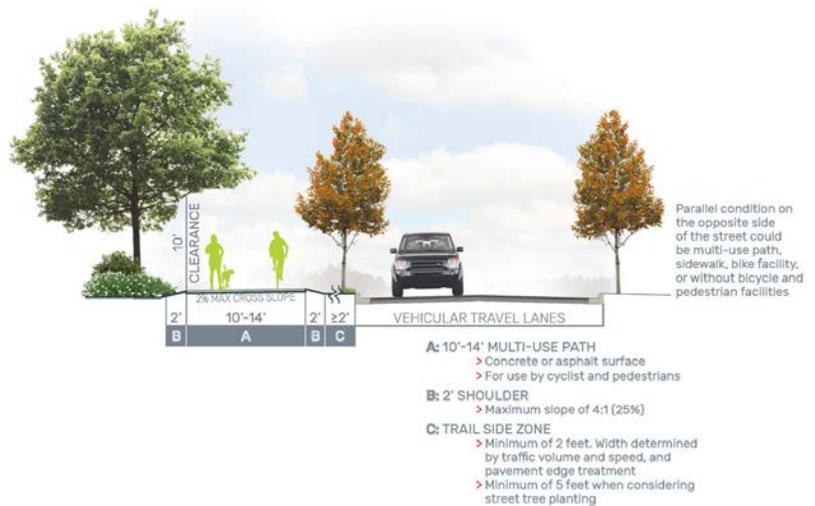
Small Town & Rural Design Guide | Physically Separated - Multi-Use Path

Greenways & Multi-Use Paths

Coddle Creek Greenway

Phase I & II, planned for a 0-5 year time frame in the Open Space Connectivity Analysis Plan, the Coddle Creek Greenway starts at Afton Village and continues to Weddington Road in Study Area A. Phase III and beyond are in the 5+ year time frame.

The plan calls for the Coddle Creek Greenway in Study Area B to follow the Creek passing under the Roberta Road and Hwy 49 bridges. A new multi-use path on Roberta Road will connect the Coddle Creek Greenway to the new multi-use path on Stough Road (future George W. Liles Pkwy extension) and Frank Liske Park.



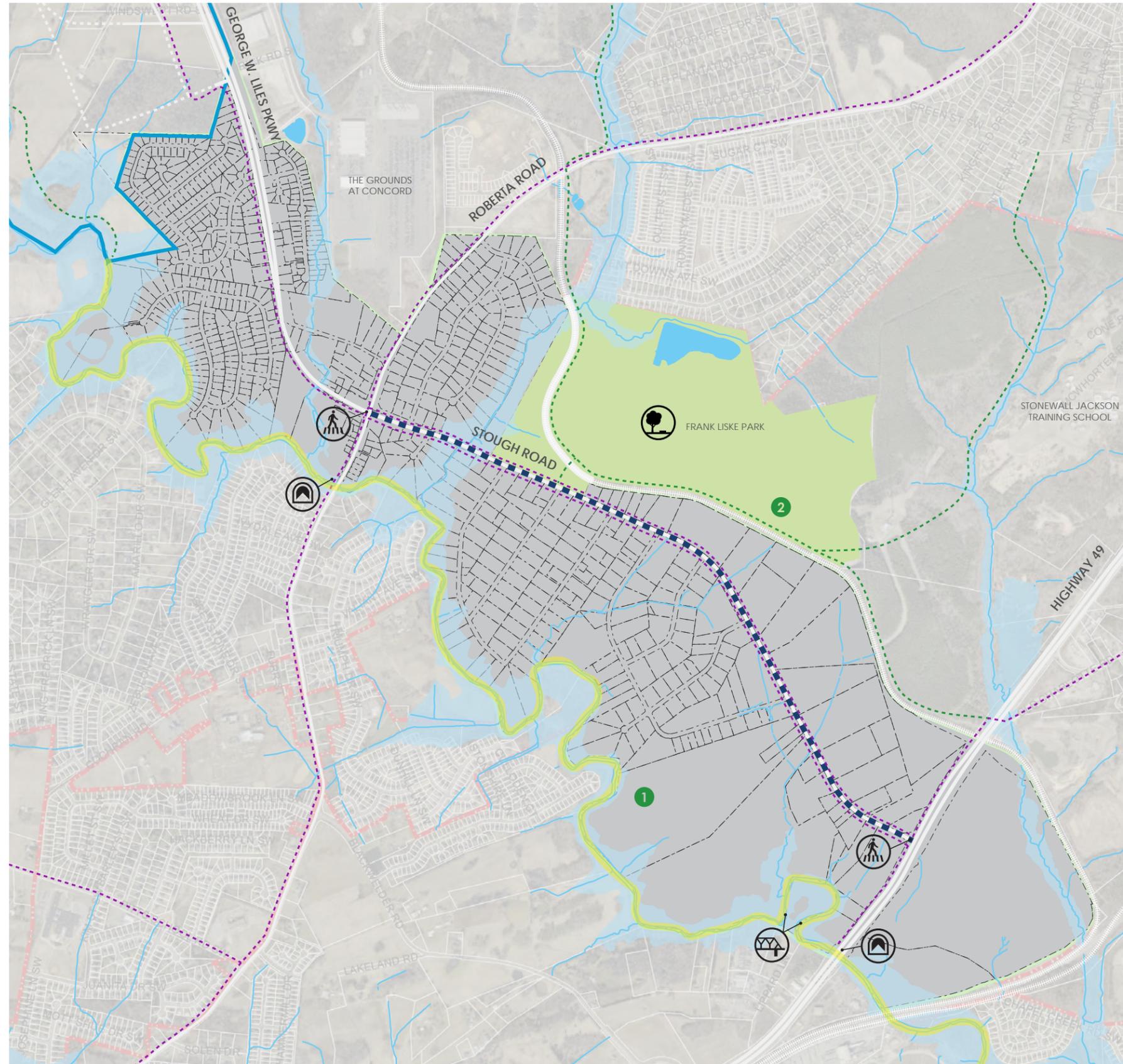
OSCA | 10'-14' Multi-Use Path with 2' Shoulder

The section of the proposed Coddle Creek Greenway, starting at the Roberta Farms Neighborhood and continuing south towards Hwy 49, is not in Parks and Recreation's 10-year plan and will be evaluated in the future for greenway considerations. The location of the greenway will be driven by the availability of land on either side of the creek. Land acquisition is the number one driver of when and where greenways are developed, if there's no clear progress on acquiring land Parks and Recreation will look at achieving connectivity through other means.

NC/Bootsmead Rail Spur Greenway

The Bootsmead Rail Spur Greenway is identified in the OSCA Plan as connecting from a new multi-use path near Hwy 49 through Frank Liske Park and The Grounds at Concord and ultimately terminating at Weddington Road/Rock Hill Church Road.

MAP 3-9: AREA B - PARKS & RECREATION RECOMMENDATIONS



- SMALL AREA PLAN BOUNDARIES
- OPEN SPACE
- WATER FEATURE
- FLOOD DATA
- GREENWAY
- PROPOSED MULTI-USE PATH
- PROPOSED CODDLE CREEK GREENWAY PER OSCA PLAN - LOCATION TBD
- # GREENWAY
- PKWY FUTURE EXT.
- EXISTING RAIL SPUR
- 🚶 CROSSWALK
- 🌳 FRANK LISKE PARK
- 🚶 TRAILHEAD
- 🏠 PEDESTRIAN TUNNEL
- 🌉 PEDESTRIAN BRIDGE

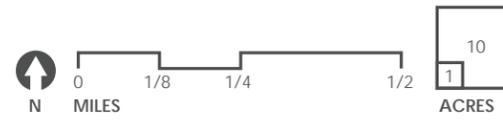
Coddle Creek Greenway
 The Open Space Connectivity Analysis Plan calls for Phase I & II planned for 0-5 year time frame and starts at Afton Village and continues to Weddington Road. Phase III and beyond, including the area within Area B, are in the 5+ year time frame.

NC/Bootsmead Rail Spur
 The Bootsmead Rail Spur Greenway is identified in the OSCA Plan as connecting from a new multi-use path near Hwy 49 through Frank Liske Park and The Grounds at Concord and ultimately terminating at Weddington Road/Rock Hill Church Road.

Roberta Road Multi-Use Path

Stough Road Multi-Use Path

Hwy 49 Multi-Use Path



Proposed Multi-Use Path Corridors

George W. Liles Pkwy Extension and future widening from north of Roberta Road to Hwy 49 proposes multi-use paths on both the north and south side of the extension. However, if only one side of the road can be accommodated, the preference is for a multi-use path to be installed on the north side of the extension.

Multi-Use Paths are also recommended for Roberta Road and Hwy 49 as shown on the Map on Page 115.

Other Recreation Facilities

Pedestrian Sidewalks

Sidewalks are dedicated to and designed for use by pedestrians. They should be safe, comfortable, and accessible to all. Sidewalks are physically separated from the roadway by a curb or unpaved buffer space and are paved. Like multi-use paths, sidewalks are typically parallel to a roadway but are designed for pedestrians only, not for bicycles or other recreational purposes.

(THIS PAGE INTENTIONALLY LEFT BLANK)



3.4. AREA B RECOMMENDATIONS

Live-Work-Play

- New village center near the George W. Liles Pkwy/Stough Road and Roberta Road Intersection.
- Encourage small scale industrial workplace opportunities along George W. Liles Pkwy extension near the existing Motorsports Industrial Park.
- Build upon the existing mix of housing opportunities throughout the study area.
- Install the Coddle Creek Greenway as recommended in the Open Space Connectivity Analysis Plan & improve connections to Frank Liske Park.

Healthy & Active Living

- Install bike facilities, parks and greenways to encourage active living.
- Install a multi-use path from Coddle Creek Greenway to Frank Liske Park as recommended in the Open Space Connectivity Analysis.
- Include future expansion plans for Frank Liske Park in the future analysis of Parks & Recreation needs in the surrounding area.

Enhanced Quality of Life

- Provide easy access from housing to workplace, dining, shopping and recreational activities via automobile, bike and/or foot.
- Ensure compatibility between neighboring land uses and provide adequate buffering between conflicting uses.
- Add more employment opportunities industries that support The Grounds at Concord.

Connected & Accessible Places

- Support NCDOT State Transportation Improvement Program (STIP) project to extend George W. Liles Pkwy all the way to Hwy 49 with a multi-use path on the each side.
- Provide safe roads for all modes of transportation includes bus, personal vehicle, bike, and foot.
- Install Public Transit Bus Route connecting Concord and Harrisburg as recommended in the Long-Range Public Transportation Mater Plan.



(THIS PAGE INTENTIONALLY LEFT BLANK)

3.5. IMPLEMENTATION

3.5.1. TYPES OF IMPLEMENTATION

Regulatory

There are two principle regulatory considerations within Area B. The first is modified industrial design standards in keeping with Motorsports Industrial Park that would apply to industrial-zoned properties currently in unincorporated Cabarrus County along the Stough Rd corridor and to the presumed future industrial property adjacent to Motorsports Industrial Park.

The second regulatory consideration applies to the Morrison Ridge Property. The Morrison Ridge property is currently categorized in the 2030 Land Use Plan as a commercial property. City staff currently anticipates a request from the private sector to change the designation to Mixed Use Activity Center. The public sector may prefer to see the property designated as Industrial/Employment, which would be more consistent with the surrounding development. The outcome of this ongoing discussion will impact future development in Area B.

Public Infrastructure

The largest public infrastructure impact to Area B is the completion of the George W Liles Pkwy extension to Hwy 49. In keeping with the lifestyle focus, there is also the potential expansion of a future greenway into Area B that will connect residents with employment and recreational opportunities.

Partnerships

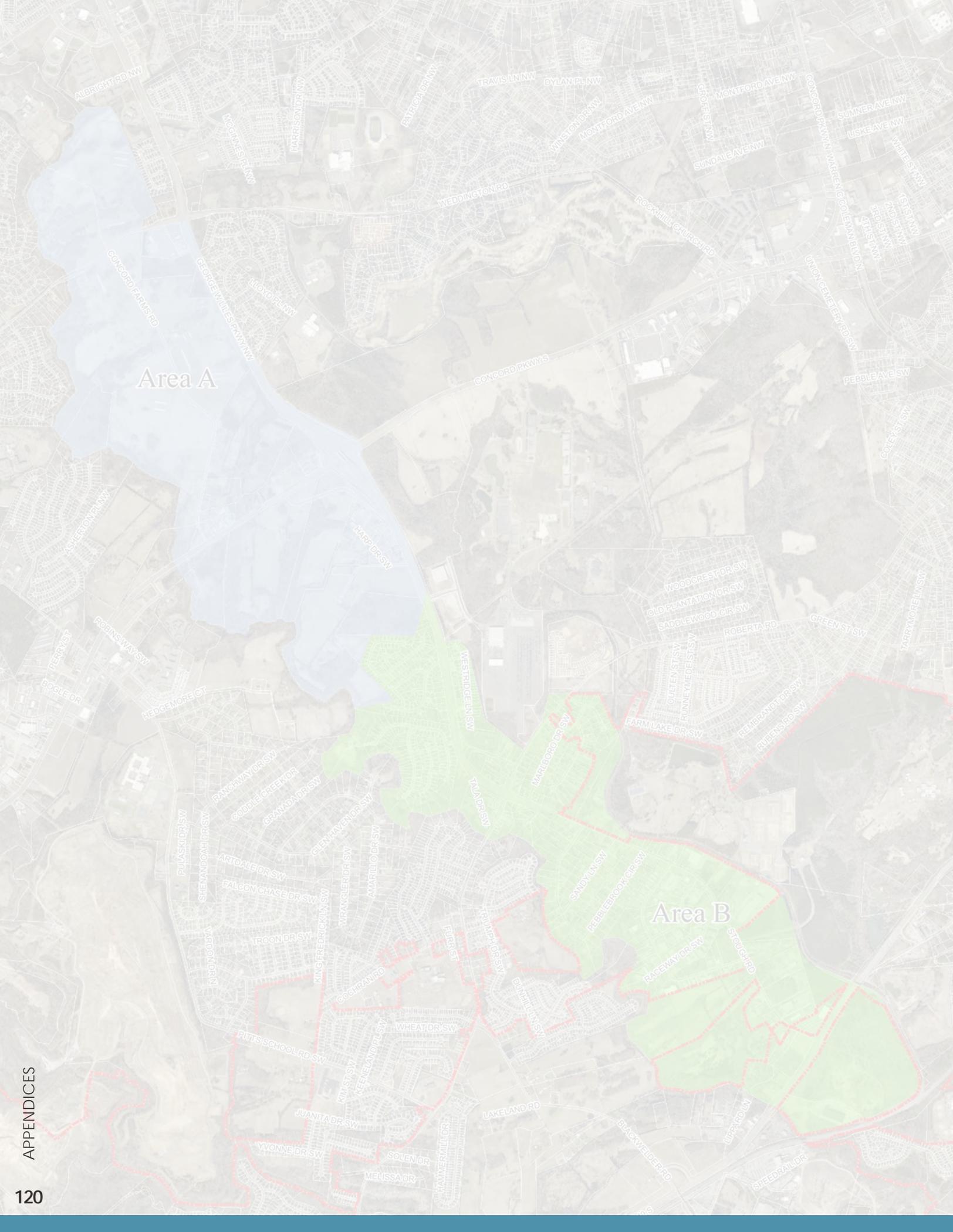
Public-private partnerships involving the City are less likely in Area B, unless the City becomes involved with the Stonewall Jackson Training Camp redevelopment. However, adjacent to Area B, the City has been involved with a sewer line extension to the Grounds at Concord property to facilitate economic development objectives. The City's economic development incentive program offers performance tax based and infrastructure incentives to qualifying industrial projects that meet minimum investment and job requirements.

3.5.2. KEY VARIABLES AND CONSTRAINTS

The Morrison Ridge property is the largest key variable within Area B. Its development marks the end of the study area and it is pivotally placed between industrial and residential uses. Its development is further complicated by lack of direct access to the property from Stough Rd. and reliance on right-in, right-out traffic flow patterns onto NC 49.

Area B, like Area A, will largely be influenced by the future build out of the Grounds at Concord property and demand for surrounding land by both supporting industries and employees seeking housing.

Area B's principal constraint is that much of the area is already developed. This may mean resistance during the entitlement process, conflicts between adjoining land uses, and limits on available land.



Area A

Area B



APPENDICES

APPENDIX A: AREA A - DEVELOPMENT GUIDELINES

APPENDIX B: AREA B - INDUSTRIAL OVERLAY DISTRICT

APPENDIX C: OPEN SPACE CONNECTIVITY ANALYSIS MAPS

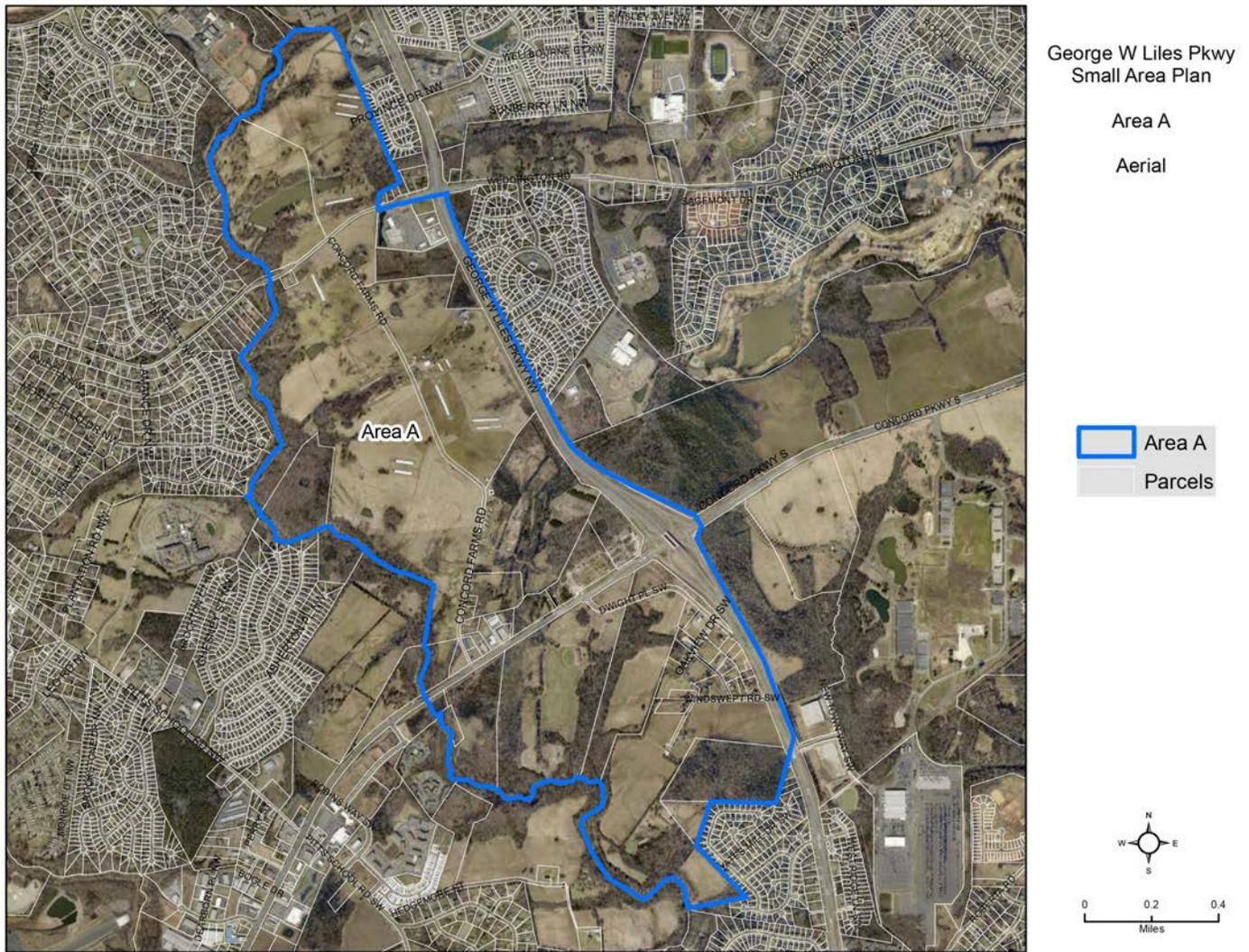
APPENDIX D: LONG RANGE TRANSIT MASTER PLAN MAPS

APPENDIX A: AREA A - DEVELOPMENT GUIDELINES

The following Development Guidelines are intended to help guide development in Study Area A of the George W. Liles Small Area Plan as shown in the blue boundary in the aerial map below.

Please reference the City of Concord's Technical Standards Manual and Concord Development Ordinance for technical requirements and further information.

FIGURE 1.0



OVERALL SITE GUIDELINES

Mixture of Land Uses

- Village Center
- Corporate Campus
- Residential
- Green Space

Site Design

All buildings should be arranged and grouped so that their primary orientation complements adjacent buildings. Adjacent buildings should relate in similarity of scale, bulk, height, architectural style, and/or configuration to existing development, as applicable, and consistent with one of the following site layouts:

- Buildings orient toward, frame and enclose a main pedestrian and/or vehicle access corridor within the development site, including an entry/spine street; or
 - Buildings orient toward, frame and enclose on at least three sides, parking areas, public spaces or other on-site amenities; or
 - Buildings orient toward and frame the corner of a perimeter or internal street intersection; or
 - Buildings orient to adjoining development, respecting adjoining exterior street alignment(s).
- For similarly used properties, the grade of adjacent lots should match where the properties meet. If there is a significant grade difference, development should create an attractive transition using grading and landscaping or a decorative retaining wall, incorporating vehicular and pedestrian cross-access.
- Sites can be divided by streets, alleys, rights-of-way, or easements, but should be so located and arranged as to permit a unified design for the overall development.
- All projects should have access to at least one major or minor thoroughfare or connection to an existing approved site having such access where Site Plan and/or Subdivision approval has been previously granted.
- Transitional buffer yards should be provided between uses that may differ in development intensity and density. These landscaped planting yards are intended to ensure that a natural area of appropriate size and density of plantings is planted or preserved between zoning districts and/or uses. For example, an area zoned I-1 Light Industrial would require a 50' wide min. landscape buffer or 25' wide min. by 6' high berm if next to a RM-2 Residential Medium Density zone.
- Unscreened concrete retaining walls or rock covered slopes (i.e. rip-rap) are not permitted. Landscaping should be placed at the base of retaining walls. Landscaping and safety railings may be required at the top of the retaining wall as well.
- Parking lot landscaping should include at least one shade tree per 10 parking spaces. If visible from the right-of-way, parking areas should be screened from view. Parking areas located in the side yard should have the portion of the lot that fronts the street screened using shrubs, brick walls (matching the adjacent building), wrought iron fencing or a combination thereof. A minimum planning area width should be no less than 4 feet.

Development Timing

The approved concept and phasing plan for development(s) in this area should include a mix of approved principal uses either in each phase or in successive phases so that no one type of use is developed exclusively.

Compatibility Guidelines

- Uses within different land use categories, can abut at rear lot lines or at side lot lines. Open space, such as parks, squares, greens and plazas should be considered a similar land use with all use categories.
- In order to provide a continuous pedestrian transition for residential neighborhoods and commercial areas, retail land use categories should not be separated from Multi-Family or Single-Family land use categories by berms, fences or buffers. Adequate design measures

should be taken to minimize potential conflicts: for example, providing sufficient screening of dumpsters, mechanical equipment, and loading areas. Limited fences and walls can be used when other design measures are ineffective.

- The following land use categories may abut at side lot lines or face across a street, square, park or common space:
 - Single Family may abut multi-family and small scale institutional or civic;
 - Multi-Family may abut single-family, office, civic, institutional or retail;
 - Retail may abut multi-family, office, civic or institutional;
 - Retail uses include shops, restaurants, entertainment and lodging.
 - Office may abut retail, institutional, civic, or multi-family.
 - Institutional may abut single family (if the institutional use is small in scale), multi-family, office, civic or retail.
 - Institutional uses include privately owned uses including religious buildings, non-profit institutions, private recreational facilities, clubhouses, museums, cultural societies, visual and performance arts buildings.
 - Civic may abut institutional, multi-family, office or retail.
 - Civic uses include governmentally owned or funded uses that include public schools, libraries, post offices, municipal offices and meeting halls. EMS, fire and police stations are also civic uses, but due to noise considerations are more restricted in their location.

ARCHITECTURAL STYLE & APPEARANCE GUIDELINES

- These criteria are not intended to restrict imagination, innovation or variety, but rather to help focus on design principles that will result in creative and cohesive approaches to architecture and building design.
- Building design should promote a diversity in style while striving to define a distinct character and maintain a high-quality development standard. New buildings should be built sensitive to the scale, form and proportion of other buildings in the same project. A human scale should be achieved at entrances to all buildings through the creative use of windows, doors, canopies and columns.
- Buildings should include similar architectural styles but should not be identical throughout the development. All sides of an individual building should be treated in an architecturally similar manner.
- Non-Residential building materials should be of a high-quality. No building elevation should be covered (exposed) with plain concrete, plain concrete block, vinyl siding, exterior panelized plywood, including foundation materials. Except, however, such materials could be used as secondary exterior materials if they cover no more than 10 percent of the surface area. Non-Residential buildings could consist of any of the following materials: utility brick, standard brick, stucco, synthetic stucco, colored split-faced block, glass, stone, tile or other similar high-quality materials. Awnings should be constructed of durable materials.
- The use of unusual shapes, colors or other characteristics that would cause a building to create aesthetically unpleasant visual disharmony with other buildings should be avoided.

Building Design

- Fenestration: Windows, doors, columns, eaves, parapets, and other building components should be proportional to the overall scale of the building.
- Underground Utilities: Underground utilities (and associate pedestals, cabinets, junction boxes and transformers) should be located in alleys, where possible. To reduce the visual impact of overhead wiring, new utility services should be located underground.

- Signs: A comprehensive sign plan should be submitted at the time of site plan submittal. Such a plan should include the location, type, size, and height of all proposed signs in accordance with the Concord Development Ordinance.
- Mechanical and Utility Equipment: Mechanical and utility equipment should be screened from view from nearby streets and properties in the same manner as trash containment areas.
- Screening Dumpsters: All dumpsters should be screened in accordance with the Concord Development Ordinance.
- Building Elevations: To avoid the visual monotony created by large, blank building elevations, the elevation must be separated by a projection or structural relief such as:
 - Constructing a porch with a roof,
 - Incorporating fascias, canopies, arcades, or other multidimensional design features to break up large wall surfaces on their street facing elevations,
 - Setting part of the facade back at least three feet from the rest of the façade
 - Creating a visually distinct ground floor,
 - Providing for changes in material or texture,
 - Installing a row of windows on the building's street facing elevation.
- George W. Liles Parkway Frontage
 - Building Elevations should not turn their back to the George W. Liles Parkway.
 - In addition to building elevation considerations, landscaping elements should be considered to enhance the entrance to the future Village Center off of the Parkway.
 - Higher density, multi-story buildings should be placed along the George W. Liles Parkway with density and building height reducing as development moves towards Coddle Creek.
- Concord Parkway (Hwy 29) Frontage
 - As a designated future High Capacity Transit route in the 2020 Cabarrus County Long Range Public Transportation Master Plan Concord Parkway is an important corridor for Concord and the surrounding area.
 - As a potential future Transit Oriented Development with the introduction of either bus rapid transit or light-rail, extra consideration should be taken as far as building elevation, building orientation and density along Concord Parkway.

Building Entrance

All buildings should be arranged and grouped so that their primary orientation complements adjacent buildings. Adjacent buildings should relate in similarity of scale, bulk, height, architectural style, and/or configuration to existing development.

MULTI-MODAL TRANSPORTATION GUIDELINES

Vehicular

- Blocks
 - Development should be arranged in a pattern of interconnecting streets and blocks, while maintaining respect for the natural landscape and floodplain.
 - Development should be based on a block structure in order to provide connectivity both within the mixed-use development and with adjacent street systems
 - Block face ranges should be designed to encourage connectivity. Lighted mid-block pedestrian pass-throughs should be considered when block faces exceed 400 feet.
 - These recommendations do not apply along State Roads.
- Circulation
 - Primary vehicle access should be provided from perimeter arterial streets and from

perimeter collector streets. Vehicle access from surrounding residential areas and adjacent neighborhoods should be provided via perimeter collector streets and adjacent local streets.

- o Internal streets should be aligned to connect with existing or planned external streets of equivalent functional classification in order to create through street connections from mixed-use development to adjacent development. Where it is necessary to prevent cut-through traffic from entering residential areas, street alignments should be discontinuous and traffic calming improvements should be utilized.
- o Internal streets provided according to this section can be public or private. All streets should be designed and constructed according to the policies, standards, and guidelines governing street design in mixed-use areas outlined in the City of Concord Technical Standards Manual.
- Street Networks
 - o Connectivity: Streets should interconnect within the development and with adjoining development. Street stubs should be provided with development adjacent to open land to provide for future connections.
 - o Cul-de-sacs: The use of cul-de-sacs in place of complete through street connections is strongly discouraged. Exceptions may apply.
 - o Main Entry: Entry/spine streets are required for the Village Center. Entry/spine streets should provide their main access from arterial streets into the center and are intended to clearly identify a driver has entered a "unique" area.
 - o Per the CDO a minimum five (5) foot sidewalk with an eight (8) foot planting strip should be included on both sides of all new interior access streets and parking area designs. Sidewalks can be constructed at the time of development or can be phased in over a period of several years as portions of a project are developed.
 - o Pedestrian Safety: Pedestrian refuge areas or medians are required on all roadways classified as major collector and above. All pedestrian crossings should comply with the standards set forth in the Americans with Disabilities Act (ADA) and/or City of Concord Technical Standards Manual, whichever is the stricter rule. Pedestrian ramps should be oriented directional only.

Bicycle

- Safe and convenient bicycle and pedestrian access from the development site should be provided to existing and designated public bike paths or greenways located on or adjacent to the development site.
- Bicycle lanes or multi-use paths should be included along collector streets where the projected motor vehicle volumes based on the traffic impact study are above 4500 vehicle trips per day and the street's posted speed limit is above 25 mph.
- Bicycle lanes or multi-use paths should also be provided on any thoroughfares in accordance with bicycle components of the adopted transportation plan bicycle master plan. Connections to adjoining bicycle paths or lanes should be made.
- Multi-use paths should be at least 10 feet in width to match Open Space Connectivity Analysis (OSCA) Plan.
- Bicycle lane widths should conform to those outlined in the City of Concord Technical Standards Manual.
- Bicycle Parking
 - o An "Inverted U" or other bicycle parking rack that supports the bicycle at two points on the bicycle frame is the minimum standard for fulfillment of the bicycle parking standards. A single "Inverted U" rack will count as two bicycle parking spaces.

- o Long term bicycle parking, which protects the entire bicycle and its components from theft, vandalism, and weather (such as bike lockers, locked rooms) can be provided for use by employees and students and can count toward fulfillment of the bicycle parking requirements.

Pedestrian

- Sidewalk & Walkway Guidelines
 - o Sidewalks and walkway should be designed according to the standards shown in the City of Concord Technical Standards Manual.
 - o Walkways should be designed to create a safe and uninterrupted pedestrian way, and should avoid frequent crossings by driveways or streets.
 - o At each point where a sidewalk or walkway crosses a street, the walkway should be clearly marked using a change in paving material and color.
 - o Sidewalks should be provided on both sides of the street excluding alleys.
 - o Internal sidewalks and walkways should be well lit and constructed to provide an unobstructed line-of-sight to other pedestrians, motor vehicles, and other site users.
- Connections from a perimeter public sidewalk system to the on-site sidewalks should be made at the same block length interval as exists within the development site.
- Connections should be made to provide direct pedestrian and bicycle travel from within the development to adjacent uses, transit stops, perimeter sidewalks, and to major pedestrian destinations located within an adjacent neighborhood. Pedestrian access should be provided by connection to any sidewalks or walkways on adjacent properties that extend to the boundaries shared with the development site. In order to provide efficient pedestrian connections to adjacent destinations, the City may require additional sidewalks, walkways, or bike paths not associated with a street, or the extension of a sidewalk from the end of a cul-de-sac to another street or walkway.
- The on-site pedestrian circulation system should provide the most efficient access route between the intended points of travel. Specifically, on-site pedestrian connections should be provided to and between the following points:
 - o The primary entrance or entrances to each building housing a principal use;
 - o Existing or planned transit stops, stations, and park-n-ride locations;
 - o Greenways or trail systems;
 - o On-site amenities
- Connections to On-Site Parking
 - o All developments served by on-site parking in surface lots or parking structures should provide either a sidewalk along the perimeter of the block or a designated pedestrian walkway through the parking lot, extending from the rows of parking furthest from the building served to either a building entrance or to a sidewalk or walkway leading to such entrance.
 - o Where an enhanced drive aisle forms the perimeter of a block, sidewalks should be provided on both sides of the drive aisle.
- Each development should provide an on-site system of pedestrian walkways and/or public sidewalks throughout the study area.
- Adjacent to a signalized street intersection, a pedestrian walkway should connect the on-site pedestrian system with the intersection and should be connected at a distance of no further than two hundred (200) feet from the intersection. Exceptions may be considered where there are no existing or planned perimeter sidewalks.
- In general, all developments should provide a unified and well-organized arrangement of buildings, service areas, parking, pedestrian, and landscaped common areas providing for

maximum comfort and convenience of residents, visitors, and employees with a minimum of conflicts with vehicles. More specifically, the following pedestrian design requirements should apply to all development in corporate campus:

- o Individual building sites within the development should include provisions for pedestrian-scale amenities such as benches, picnic tables, courtyards, plazas, water attractions and trash receptacles. These enhancements are essential to creating an efficient and functional environment as well as promoting a “sense of place.” Such area(s) can include covered malls for general pedestrian use, exterior walkways, outdoor seating areas, and the like where the facilities are available for common use by employees and visitors. Required open space, buffer areas and setback yards as well as improved deck, patio and roof areas can be used to meet this requirement.
- o All buildings or building clusters within the development should be connected with linkages other than roads (sidewalks, bikeways and walking paths). Unless topographic or other physical constraints exist, these linkages should be provided between adjacent sites whether developed or undeveloped. Pedestrian access can be provided at any suitable locations within the district, but should, where practicable, be separated from vehicular access points in order to reduce congestion, marginal friction, and hazards, except where signalization is used in such a manner as to control pedestrian and vehicular movements safely.
- o Loading and maintenance areas should be so located and arranged as to prevent interference with pedestrian movement within the site. All loading should be from the rear or side of the building, but not facing a public street. If rear or side loading cannot be accommodated due to site constraints measures should be taken to provide safe, efficient, and aesthetically pleasing loading areas on a public street.
- o All on-site utilities (electrical, telephone, etc.) should be located underground.
- Paving materials in pedestrian areas (including crosswalks and sidewalks) should only include brick, concrete (aggregate exposed finish), concrete pavers, brick pavers and similar materials.

LAND-USE GUIDELINES

Village Center

- A mixed-use walkable Village Center is a central component to this small area plan.
- Multi-story buildings situated along a main street forming a central campus environment.
- Primary access to the site would be via George W. Liles Parkway and Concord Farms Road (from Weddington Road & Concord Parkway).
- On street parking with rear parking lots and/or parking structures for tenants, visitors and employees.
- Parking structures should be utilized for higher density development. The use of parking structures allows the buildings to be closer together increasing the urbanism along the main street and encouraging greater pedestrian accessibility.
- Where possible parking decks should be “wrapped” with residential, office or retail construction.

Corporate Campus - North of Hwy 29

- Corporate Campus development to include a high-quality mixture of employment and/or institutional uses of varying types in a single coordinated development.
- Corporate Campus could include light manufacturing, office, institutional and limited retail and service uses in an attractive corporate park setting with architectural design standards,

landscaping, screening and buffering.

- It is not intended that development in this area be used to accommodate single-use, single building developments. Emphasis will be placed on the project's relationship to existing and future public facilities such as roads and greenways.

Industrial Development - South of Hwy 29

- This unique location within Study Area A with ease of access to George W. Liles Pkwy and I-85 lends itself to several workplace focused possibilities including but not limited to high-quality manufacturing, mixed-use development, or corporate campus development.
- Industrial development along the Hwy 29 corridor should not include a single-user building logistics or distribution user.
- If included, commercial outparcels along Hwy 29 should be integrated into the overall development of the site.

Residential

- Residential development in this area should include a wide variety of housing including large and small single-family lots, townhomes, loft apartments, condominiums as well as senior housing.
- No single lot size should comprise the majority of the lots in the development. Lot sizes should vary to allow for a wide variety of housing types.
- Housing should be high-quality, incorporating durable materials and appropriate detailing.
- Size of the area makes it possible to provide housing at many different "price points" to reach higher income levels (particularly backing up to Coddle Creek and north of Weddington Road) as well as those that are more modest.
- Affordable housing should be included that provides affordable and workforce housing needed to support the surrounding area. Affordable housing could be exchanged for higher density development.
- Density similar to the nearby Afton Village Neighborhood may be higher than the surrounding subdivisions however, the intent is that a residential area of this size is supported by and supports a vibrant mixed-use core.

Green Space

- Green space serves as the organizing armature of the area with a greenway trail along Coddle Creek. Large natural areas are to be preserved along the floodplain and pocket parks should be included throughout the development.
- Possible to include more useable green space in exchange for higher density development that provides the walkability needed to differentiate this area from the surrounding subdivisions. Density allowances will be determined on a case-by-case basis.
- Opportunities for residents to walk and bike as an alternative to using their vehicles should be provided wherever possible.
- Common accessible open space should be required for all sites, whether subdivided, or not. The open space should be pedestrian oriented and include such amenities as park benches, walking trails and gazebos. Parking or vehicular access within these areas should be prohibited.

Prohibited Uses

The following are prohibited uses within Study Area A of the George W. Liles Pkwy Small Area Plan boundary (See Figure 1.0).

- Automatic food and drink vending machines placed outside;

- Chemical manufacturing, storage or distribution as a primary use;
- Enameling, painting or plating, except artist's studios;
- Outdoor advertising or billboard as a principal use;
- Carting, moving or hauling terminal are yard, except delivery goods to businesses within Study Area A;
- Correctional institutions, detention centers or halfway house;
- Manufacture, storage, or disposal of hazardous waste materials;
- Scrap yards;
- Manufactured homes;
- Sand, gravel, or other mineral extraction;
- Kennels;
- Any use or business controlled under the Adult Entertainment use category;

(THIS PAGE INTENTIONALLY LEFT BLANK)

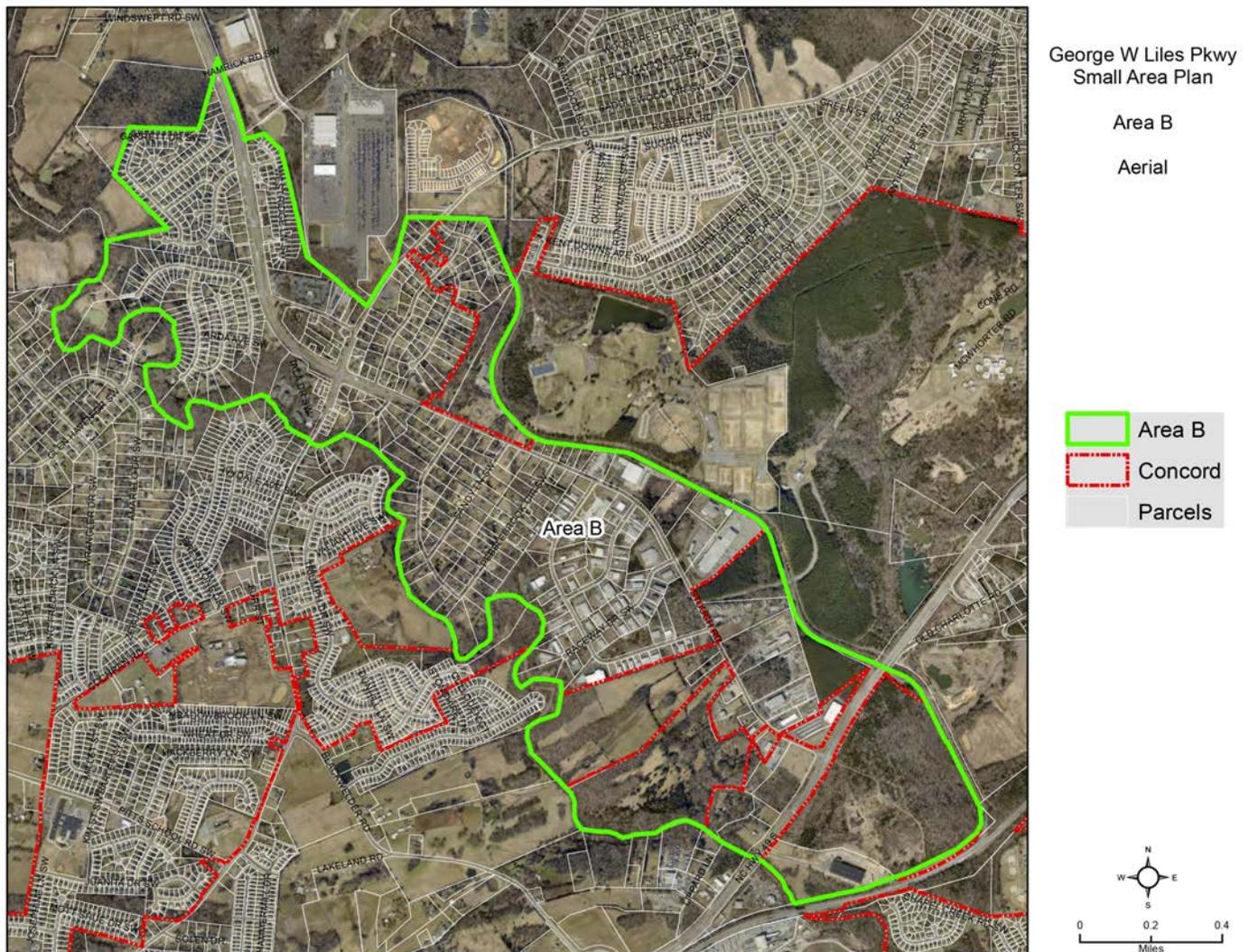
APPENDIX B: AREA B - INDUSTRIAL OVERLAY DISTRICT

PURPOSE AND INTENT

The purpose of an Industrial Overlay District for the existing Industrial Zoning Districts within the boundary of Area B as shown in the green boundary below is to modify the design standards required on Industrial Uses to match more closely with the existing industrial establishments along Motorsports Drive. Specifically, these standards are intended to create a cohesive community aesthetic (through the use of good building design, landscaping, and signage), maintain and strengthen property values, promote Concord as a high quality community, and implement the goals and polices of the Concord Land Use Plan.

If adopted this Overlay District would be incorporated into the Concord Development Ordinance.

FIGURE 1.0



APPLICABILITY

New Construction - This Section applies to all new construction in I-1 and I-2 districts. Modifications to individual requirements of this Section may be allowed on a case-by-case basis for buildings not fronting on a major or minor thoroughfare or infill development upon finding that the purposes of this section are achieved and the modification results in a project of equal or better design quality. The burden of proof relative to design quality shall rest with the applicant.

Additions to Existing Development - Additional construction that is 25 percent or less of the floor area of the existing building shall be subject only to the building setbacks and minimum parking standards. If additional parking is required, parking lot yard landscaping shall be required as set forth in the Concord Development Ordinance. Additional construction that is more than 25 percent of the floor area of the existing building shall be subject to all of the requirements of this Section. The City may allow modifications to individual requirements of this Section on a case-by-case basis upon finding that the purposes of this section are achieved and the modification results in a project of equal or better design quality that is compatible with existing development in the vicinity of the project. The burden of proof relative to design quality shall rest with the applicant.

Redevelopment - Redevelopment or remodeling that involves construction costs equal to or greater than 50 percent of the assessed value of site improvements according to the most recent property tax rolls shall be brought into conformance with the Concord Development Ordinance "Supplemental Design Standards and Requirements for Commercial Districts" except existing nonconformities due to existing roof lines and the existing locations of walls and parking areas may be retained. The City may modify individual requirements of this Section on a case-by-case basis upon finding that the purposes of this section are achieved and the modification results in a project of equal or better design quality. The burden of proof relative to design quality shall rest with the applicant.

SIGN REGULATIONS

Signs shall be regulated in accordance with standards set forth the Sign Standards Article of the Concord Development Ordinance, or by an approved comprehensive sign package.

ELECTRICAL AND SERVICE EQUIPMENT

HVAC and similar types of incidental machinery or equipment such as trash receptacles, dumpsters, utility meters, above-ground tanks, satellite dishes and antennas shall be screened from view from public streets that are external to the development project, internal collector or arterial streets, abutting residential zoning districts, and public parks.

ARCHITECTURAL PLANS

Architectural plans shall depict architectural details outlined in this Section and shall consist of:

- Preliminary renderings of building elevations plus typical cross sections to clearly define the character of the project; and
- Exterior building materials inventory to indicate compliance with this Section.

APPROVALS

The City shall review all site plans for industrial development. The City may modify the specific provisions of the design standards of this Overlay District upon finding that the alternative design is

consistent with other sites within the same industrial development and the purposes of the applicable provision are achieved through the alternative design.

COMPLIANCE

Architectural details and/or drawings shall be submitted to the City at the time of site plan submittal. All requirements of this Overlay District must be met before a Certificate of Compliance (COC) can be issued.

COMPATIBILITY

Buildings that are visible from streets abutting the development and from internal collector and arterial streets shall have compatible scales, bulk, heights, architectural styles, and roof styles. Development shall use a combination of plantings, buffers and building design to ensure compatibility with adjacent development.

BUILDING AND SITE DESIGN STANDARDS

The following design elements shall be incorporated in architectural and site plan:

Building Entrance

Primary building entrances shall be clearly defined and shall be recessed or framed by a sheltering element such as an awning, arcade, overhangs, or portico (in order to provide weather protection for pedestrians). Public entrances flush with the building wall with no cover are not allowed. (See Figure 1.1)

Boulevard Entrance

For developments containing total floor area in excess of 250,000 square feet, the principal access into the site shall be a boulevard incorporating a 10-foot wide landscaped center median for a depth of at least 100 feet in depth.

Transit Stops

Transit bus stop may be required if the development is located on an existing or planned public transit route, which shall be determined by transit plans and the Transit Director.

Sidewalks

Sidewalks shall be provided along public streets that provide access to the development. These interior sidewalks shall be constructed in accordance with the standards for sidewalks as set forth in the Technical Standards Manual.

Exterior Materials

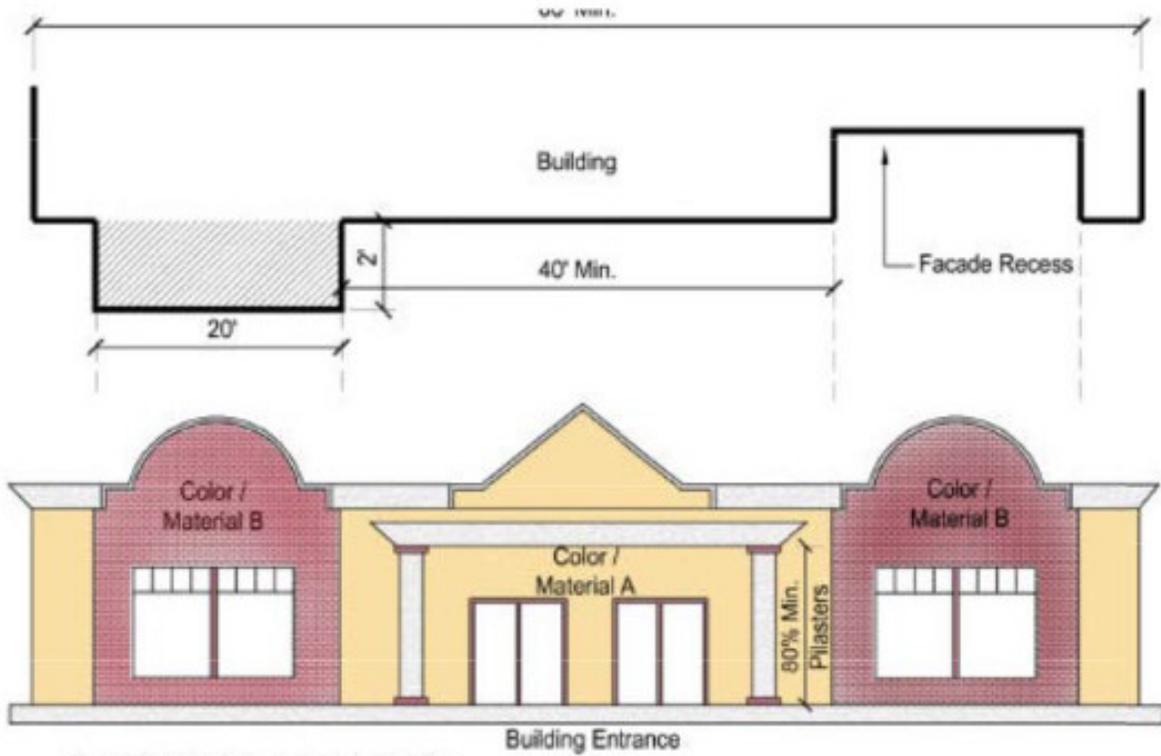
Exterior building materials may include painted tilt-up concrete, brick, stone, stucco, synthetic stucco, metal wall panels, or cement-board or wood siding. The City may approve alternative materials of equal or better quality and durability.

Metal may be utilized exclusively on side and rear exterior walls. Front façade must include brick or stone wainscot with lighter material (e.g., stucco) above or be constructed entirely of brick, stone, cement-board or wood siding. Exception if brick or stone wainscot is extended around all exterior walls then metal panels can be included on the front façade.

Parapet Walls

Average parapet height shall not exceed 15 percent of the supporting wall height. Maximum parapet height shall not exceed 33 percent of the supporting wall height. (See Figure 1.1)

FIGURE 1.1



Note: Material changes may be used in lieu of or in addition to facade offsets

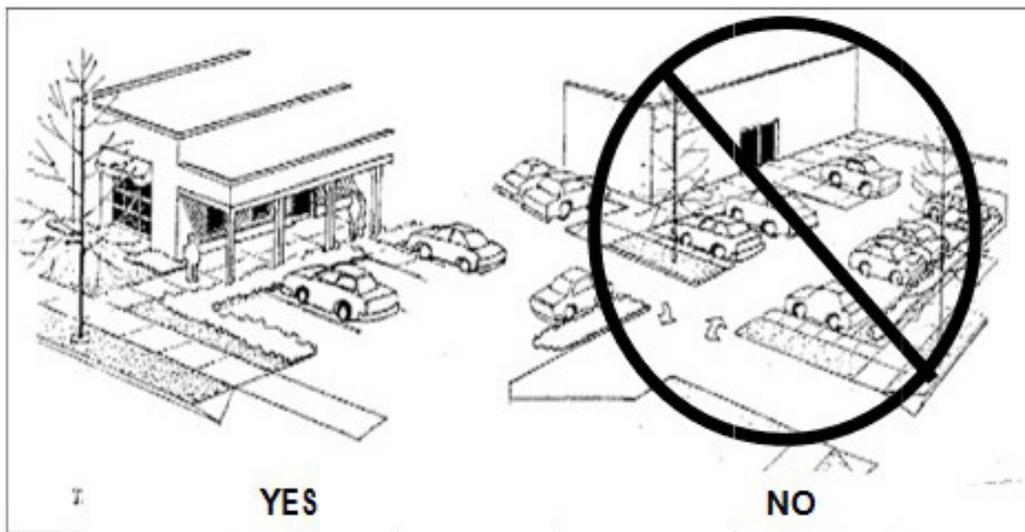
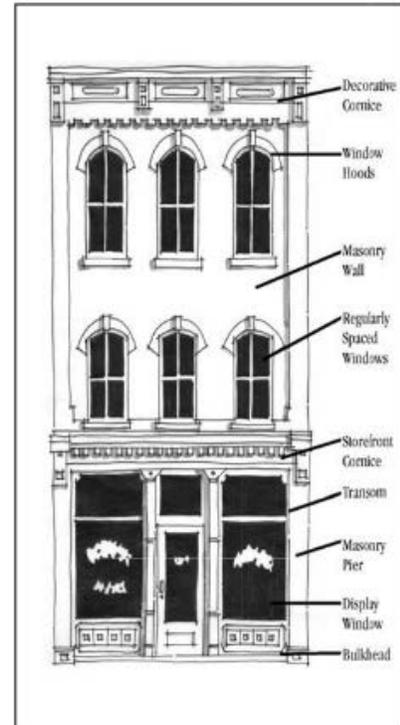
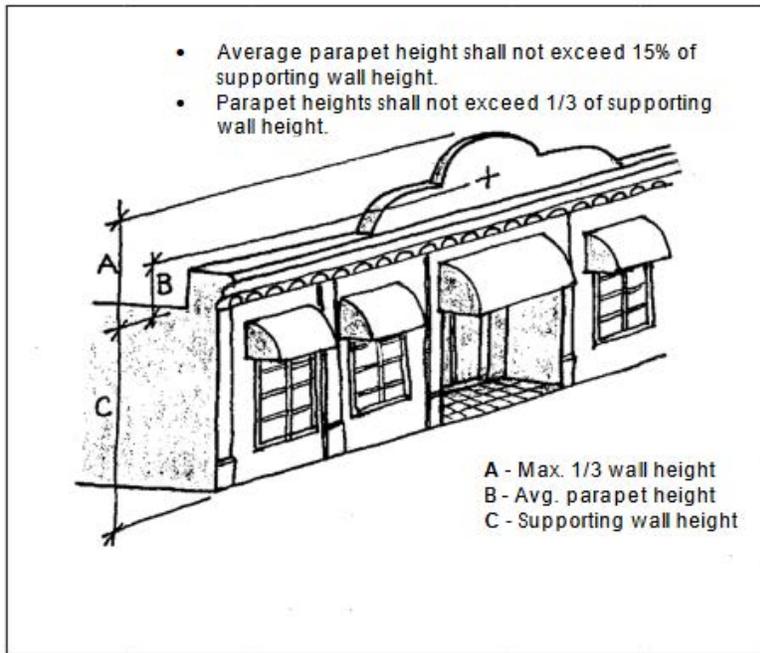
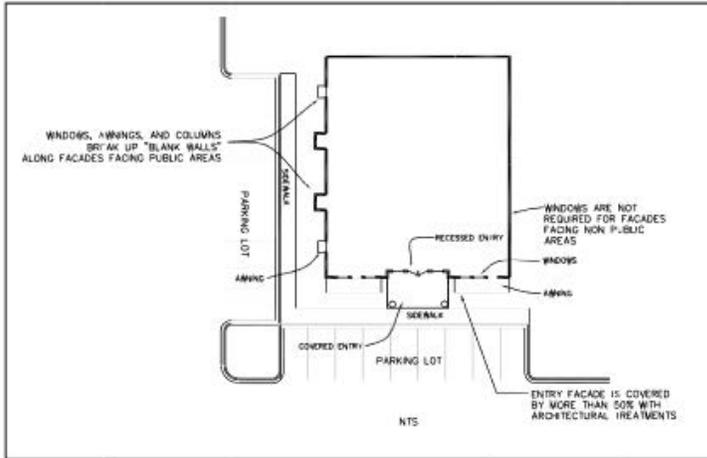


FIGURE 1.1 (CONTINUED)



Facade/Wall Treatment

Front facades shall have a recognizable "base" consisting of (but not limited to): walls, ledges, sills, integrally textured materials (such as stone or other masonry), integrally colored and patterned materials, or planters. Front facades shall also have a recognizable "top" consisting of (but not limited to): cornice treatments with integrally textured materials (other than colored "stripes" or "bands"), sloping roofs (with overhangs and brackets), or stepped parapets.

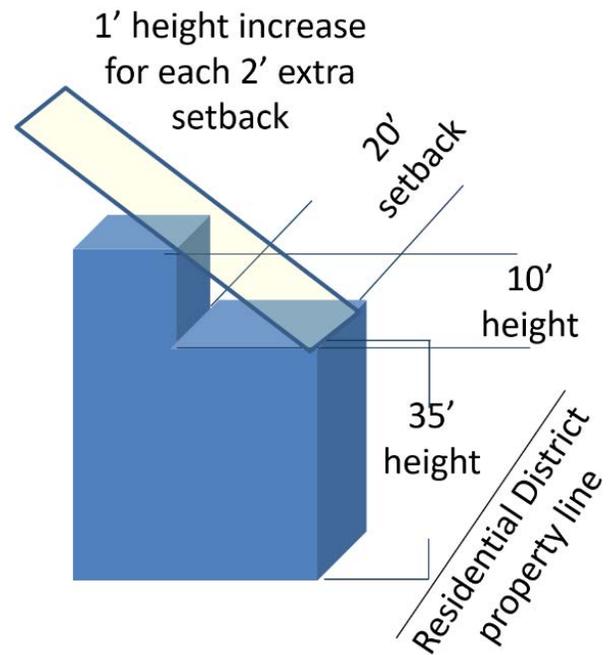
Design Elements

For building facades that are visible from streets external to a development or from internal collector or arterials streets, at least four (4) of the following elements must comprise 15 percent of front façade length:

- Parapets
- Cornices
- Roof line offsets
- Windows or doors
- Window hoods
- Transoms
- Bulkheads
- Awnings or canopies
- Ribs or columns
- 10.Changes in texture or masonry

Height Transitions

On parcels that abut residential zoning districts, portions of the building exceeding thirty-five (35) feet shall be setback an additional two (2) feet from the required building setback for each foot of height in excess of thirty-five (35) feet. For purposes of this provision, the height transition shall be measured from average grade along the nearest building line facing any property line of a residentially zoned lot to the highest point of the nonresidential structure.

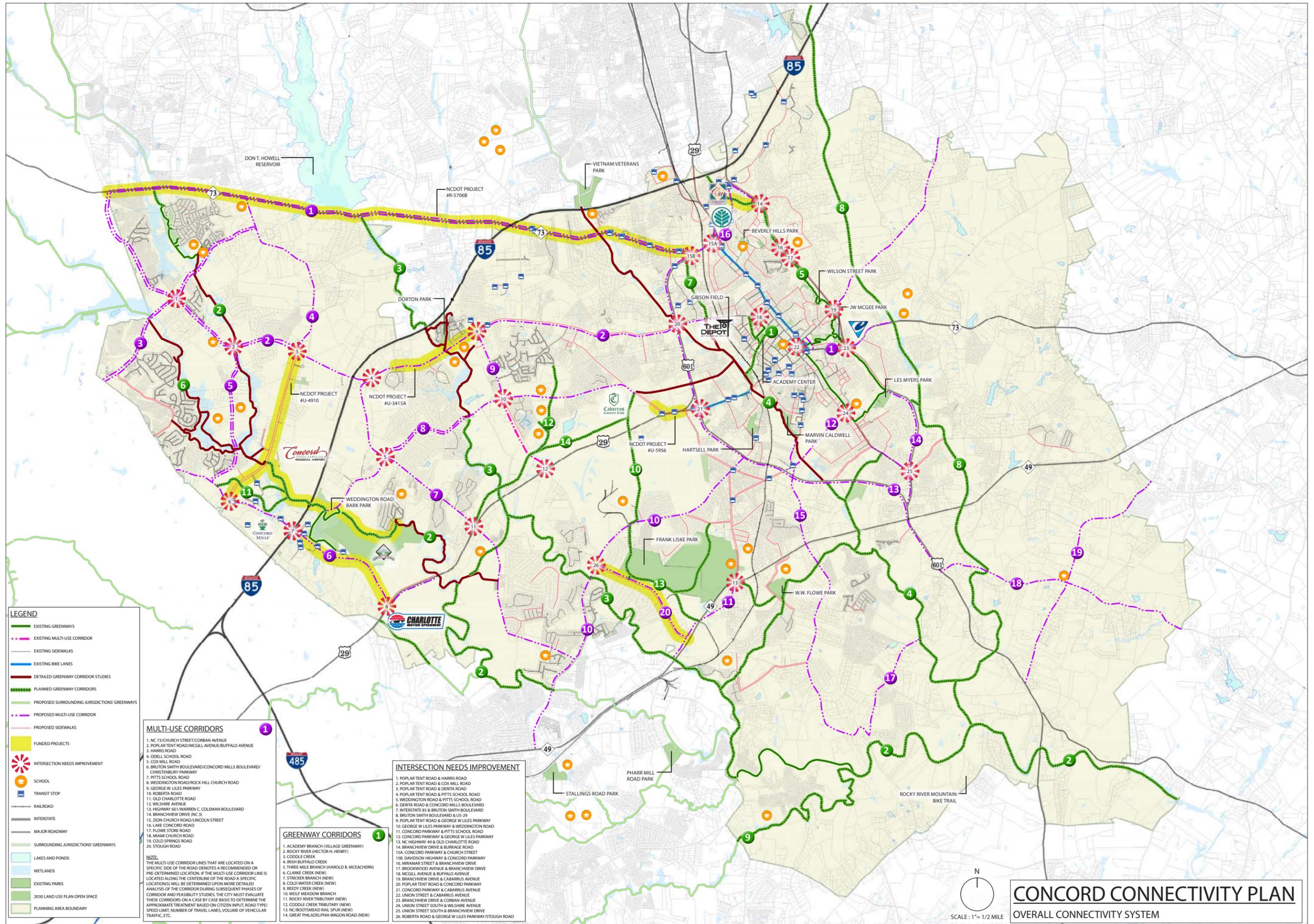


APPENDIX C: OPEN SPACE CONNECTIVITY ANALYSIS MAPS

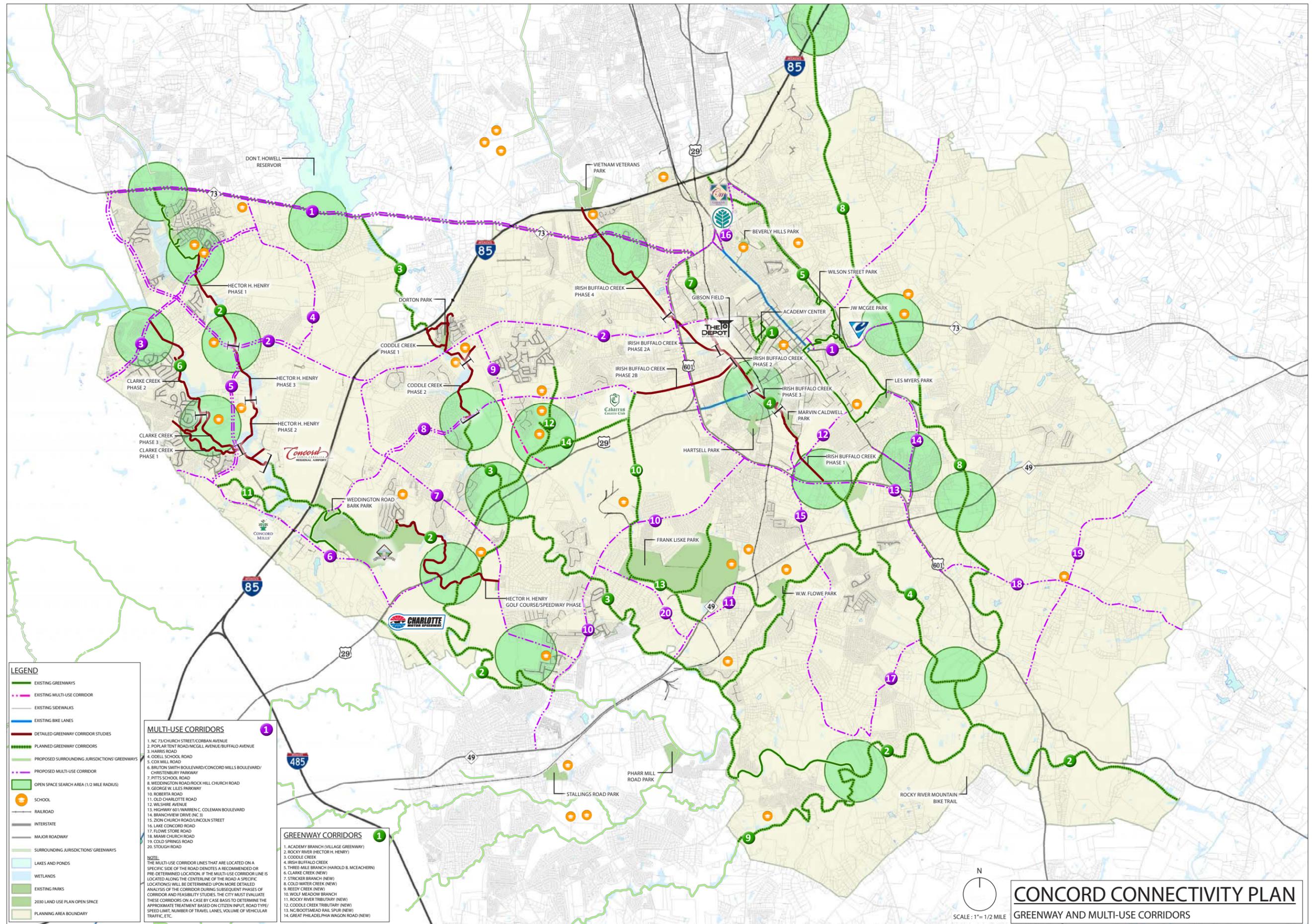
Page 139: Concord Connectivity Plan - Overall Connectivity System Map

Page 141: Concord Connectivity Plan - Greenway and Multi-Use Corridors Map

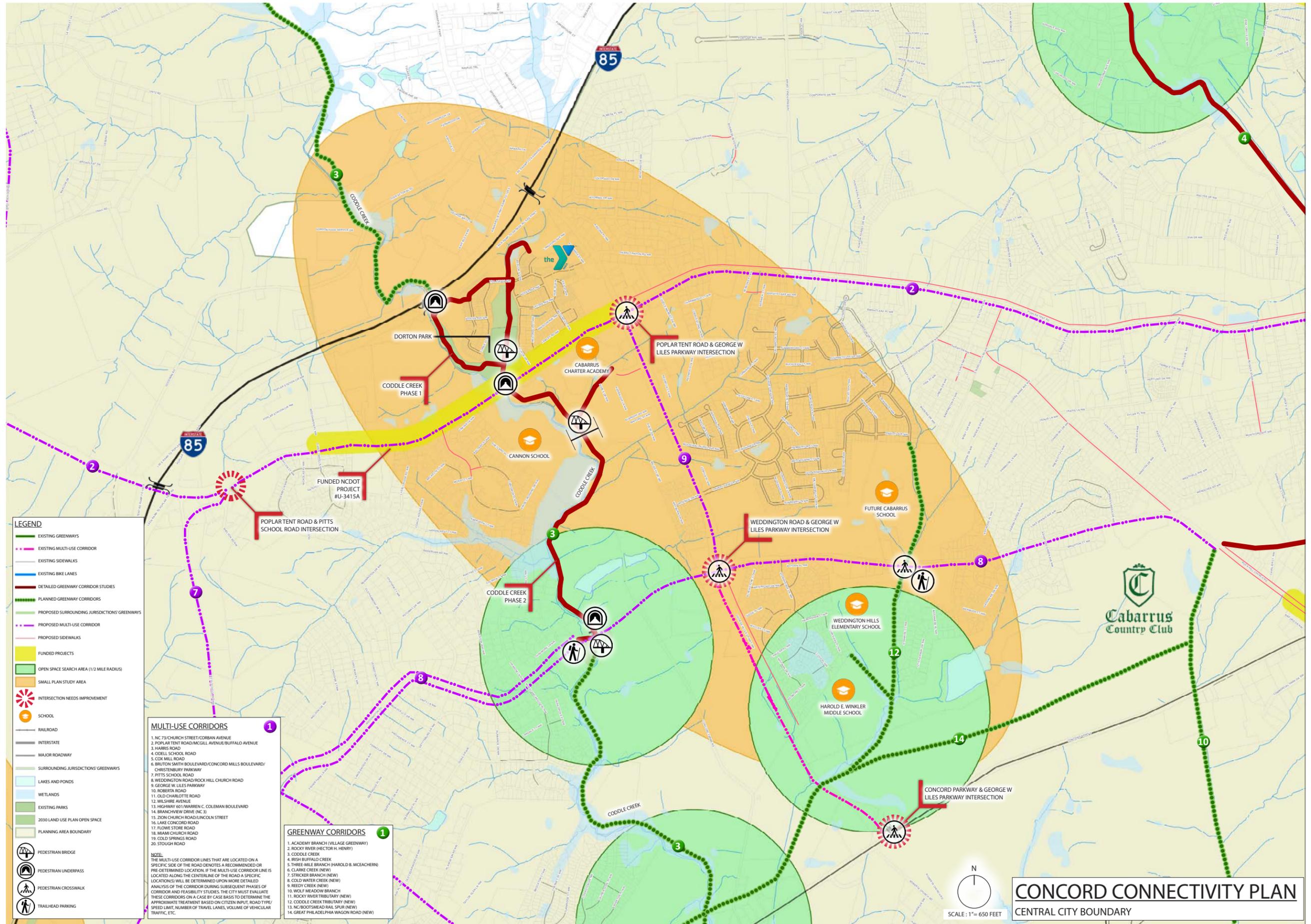
Page 143: Concord Connectivity Plan - Central City Boundary Map



(THIS PAGE INTENTIONALLY LEFT BLANK)



(THIS PAGE INTENTIONALLY LEFT BLANK)



LEGEND

- EXISTING GREENWAYS
- EXISTING MULTI-USE CORRIDOR
- EXISTING SIDEWALKS
- EXISTING BIKE LANES
- DETAILED GREENWAY CORRIDOR STUDIES
- PLANNED GREENWAY CORRIDORS
- PROPOSED SURROUNDING JURISDICTIONS' GREENWAYS
- PROPOSED MULTI-USE CORRIDOR
- PROPOSED SIDEWALKS
- FUNDED PROJECTS
- OPEN SPACE SEARCH AREA (1/2 MILE RADIUS)
- SMALL PLAN STUDY AREA
- INTERSECTION NEEDS IMPROVEMENT
- SCHOOL
- RAILROAD
- INTERSTATE
- MAJOR ROADWAY
- SURROUNDING JURISDICTIONS' GREENWAYS
- LAKES AND PONDS
- WETLANDS
- EXISTING PARKS
- 2030 LAND USE PLAN OPEN SPACE
- PLANNING AREA BOUNDARY
- PEDESTRIAN BRIDGE
- PEDESTRIAN UNDERPASS
- PEDESTRIAN CROSSWALK
- TRAILHEAD PARKING

- MULTI-USE CORRIDORS**
1. NC 73/CHURCH STREET/CORBAN AVENUE
 2. POPLAR TENT ROAD/MCGILL AVENUE/BUFFALO AVENUE
 3. HARRIS ROAD
 4. ODELL SCHOOL ROAD
 5. COX MILL ROAD
 6. BRITTON SMITH BOULEVARD/CONCORD MILLS BOULEVARD/CHRISTENBURY PARKWAY
 7. PITTS SCHOOL ROAD
 8. WEDDINGTON ROAD/ROCK HILL CHURCH ROAD
 9. GEORGE W. LILES PARKWAY
 10. ROBERTA ROAD
 11. OLD CHARLOTTE ROAD
 12. WILSHIRE AVENUE
 13. HIGHWAY 601/WARREN C. COLEMAN BOULEVARD
 14. BRANCHVIEW DRIVE (INC. 3)
 15. ZION CHURCH ROAD/LINCOLN STREET
 16. LAKE CONCORD ROAD
 17. FLOWE STORE ROAD
 18. MIAMI CHURCH ROAD
 19. COLD SPRINGS ROAD
 20. STOUGHTON ROAD

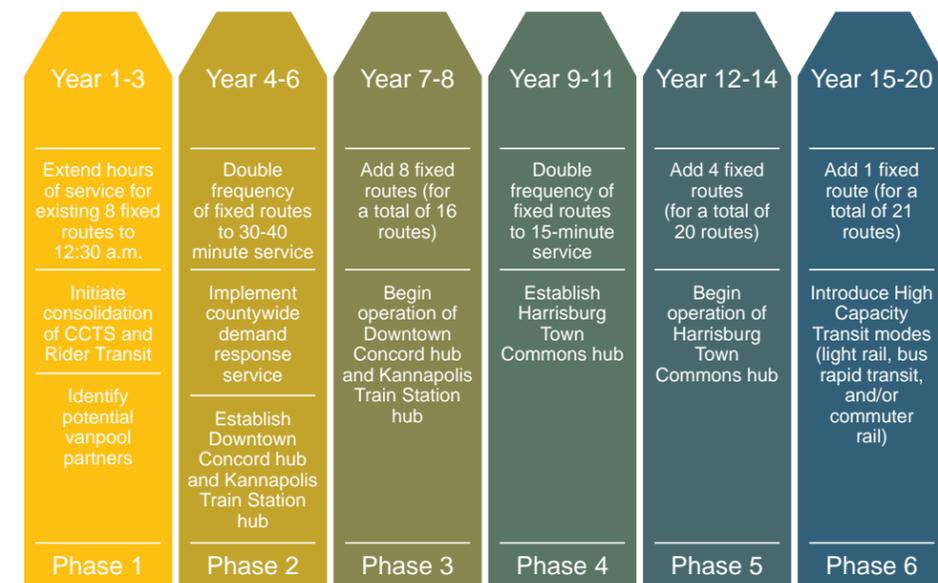
- GREENWAY CORRIDORS**
1. ACADEMY BRANCH (VILLAGE GREENWAY)
 2. ROCKY RIVER (HECTOR H. HENRY)
 3. CODDLE CREEK
 4. IRON BUFFALO CREEK
 5. THREE-MILE BRANCH (HAROLD B. MCEACHERN)
 6. CLARKE CREEK (NEW)
 7. STRICKER BRANCH (NEW)
 8. COLD WATER CREEK (NEW)
 9. REEDY CREEK (NEW)
 10. WOLF MEADOW BRANCH
 11. ROCKY RIVER TRIBUTARY (NEW)
 12. CODDLE CREEK TRIBUTARY (NEW)
 13. NC ROCKS/HEAD RAIL SPUR (NEW)
 14. GREAT PHILADELPHIA WAGON ROAD (NEW)

NOTE: THE MULTI-USE CORRIDOR LINES THAT ARE LOCATED ON A SPECIFIC SIDE OF THE ROAD DENOTES A RECOMMENDED OR PRE-DETERMINED LOCATION. IF THE MULTI-USE CORRIDOR LINE IS LOCATED ALONG THE CENTERLINE OF THE ROAD A SPECIFIC LOCATION(S) WILL BE DETERMINED UPON MORE DETAILED ANALYSIS OF THE CORRIDOR DURING SUBSEQUENT PHASES OF CORRIDOR AND FEASIBILITY STUDIES. THE CITY MUST EVALUATE THESE CORRIDORS ON A CASE BY CASE BASIS TO DETERMINE THE APPROPRIATE TREATMENT BASED ON CITIZEN INPUT, ROAD TYPE/SPEED LIMIT, NUMBER OF TRAVEL LANES, VOLUME OF VEHICULAR TRAFFIC, ETC.

CONCORD CONNECTIVITY PLAN
CENTRAL CITY BOUNDARY

SCALE: 1" = 650 FEET

APPENDIX D: LONG RANGE TRANSIT MASTER PLAN MAPS



(THIS PAGE INTENTIONALLY LEFT BLANK)

Page 145 & 146: CCX - Concord Charlotte Express

The CCX operates in a loop fashion using US 29 and I-85 currently and in Phase 1 of the LRTP. This routing allows the CCX to operate on an already tight 60-minute cycle, but as a result does not serve any additional stops except the Rider Transit Center and J.W. Clay Light Rail Station on both outbound and inbound trips. In Phase 3, it is recommended that the CCX be aligned to function as a bi-directional route on US 29 and operate as a local route serving the same stops on outbound and inbound trips as route 208. This recommended route would operate on a 90-minute cycle, thereby requiring three vehicles to achieve 30-minute frequency.

Page 147: Route 302 – George W. Liles Parkway

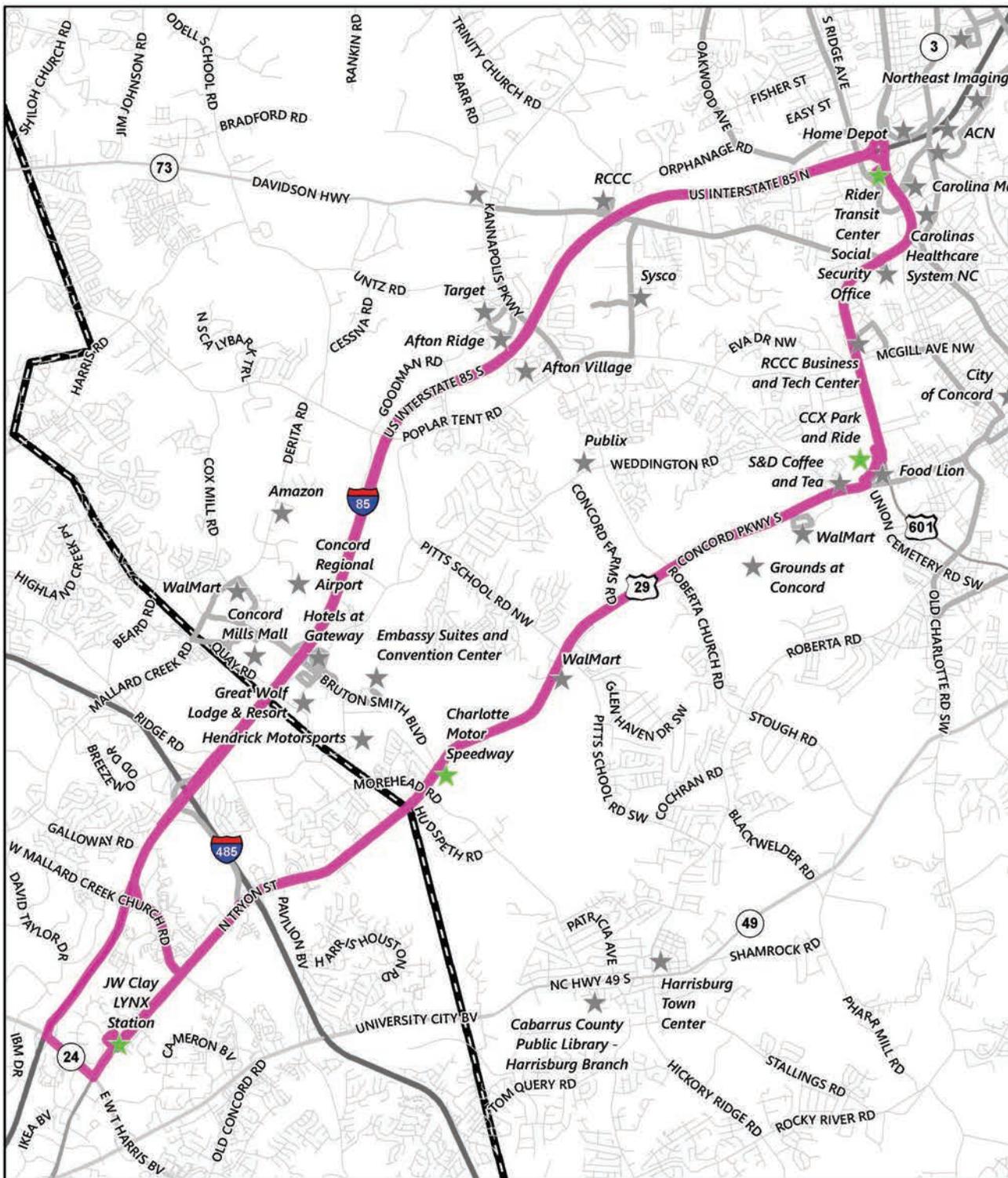
In order to address the growing transit need along the George W. Liles corridor, Route 302 is recommended to serve the area, which currently has very limited Fixed Route service. Route 302 would depart from the Downtown Concord hub, following Cabarrus Avenue to Concord Parkway, serving the Grounds at Concord, and then to George W. Liles Parkway. Route 302 would connect with multiple routes and turn around at the Afton Ridge Target. This route would be a bidirectional route and would also serve the Walmart on Concord Parkway. It is recommended, as part of the George W. Liles Pkwy Small Area Plan that Route 302 be modified to follow Concord Farms Road connecting from US 29 to Weddington Road and then George W. Liles Parkway.

Page 148: Route 304 – Connections with Harrisburg

Route 304 is recommended in Phase 5 connecting Harrisburg with Concord and Charlotte. Route 304 would depart from the Downtown Concord transit hub, taking Union Street south across Highway 49 to the Southgate Commons Shopping Center on US Highway 601. Union Street is another corridor that does not have Fixed Route transit service today, but Rider Transit has received requests for providing some level of Fixed Route service since the inception of service in 2004. Service on Union Street was also requested through the LRTP public engagement process. Southgate Commons Shopping Center could be identified as a park and ride lot for commuters. The route would then continue down Highway 49, stopping at the Harrisburg transit hub at Town Center. After Town Center, Route 304 would continue on NC 49 and arrive at the University City LYNX Station.

APPENDIX D: LONG RANGE TRANSIT MASTER PLAN MAPS

CCX - CONCORD CHARLOTTE EXPRESS



CCX

Start Phase: 1

— CCX

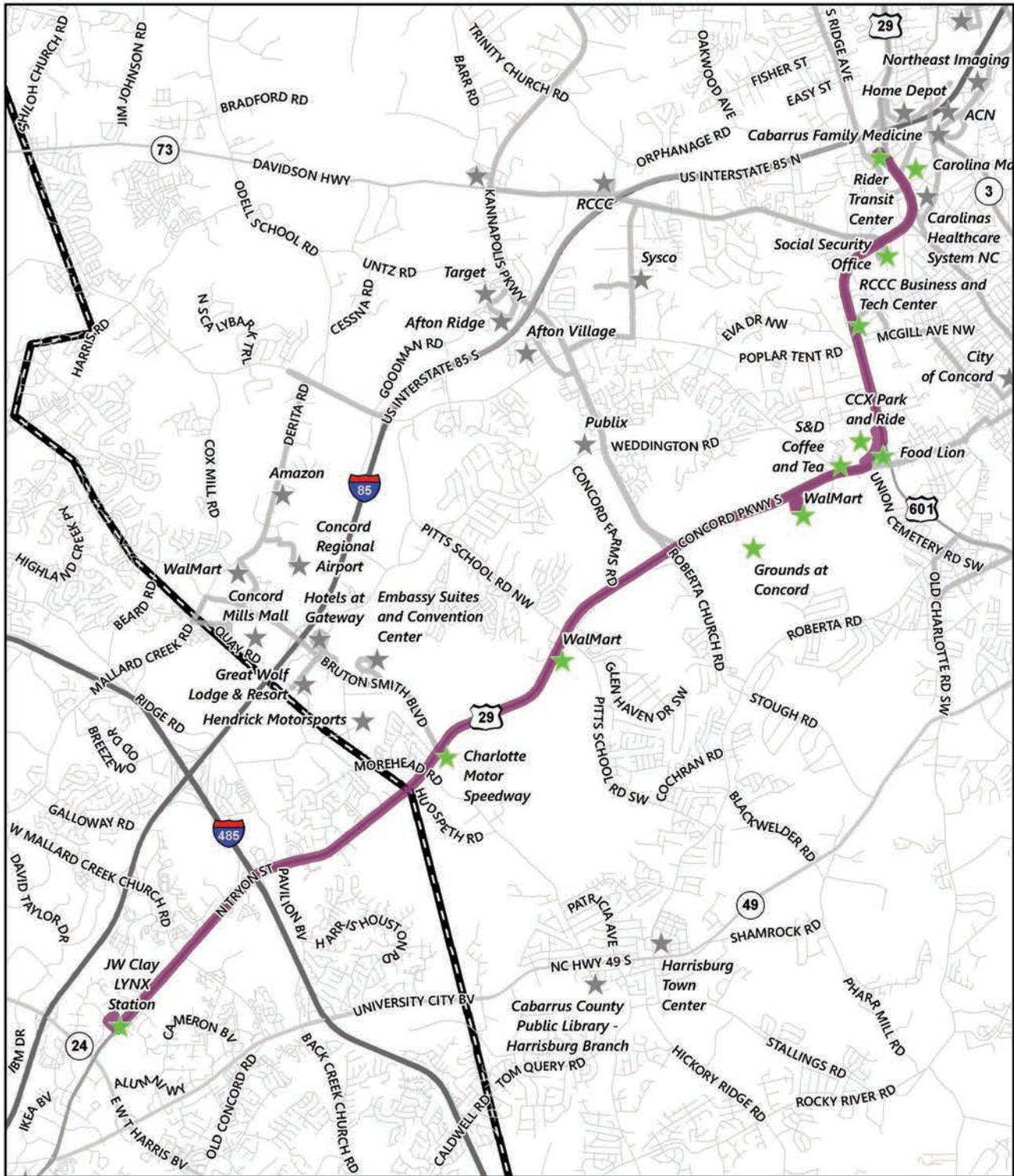
— Transit Service

★ Activity Center

▭ County Boundary



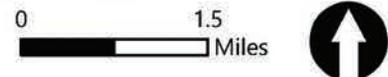
ROUTE 208 - CCX LOCAL



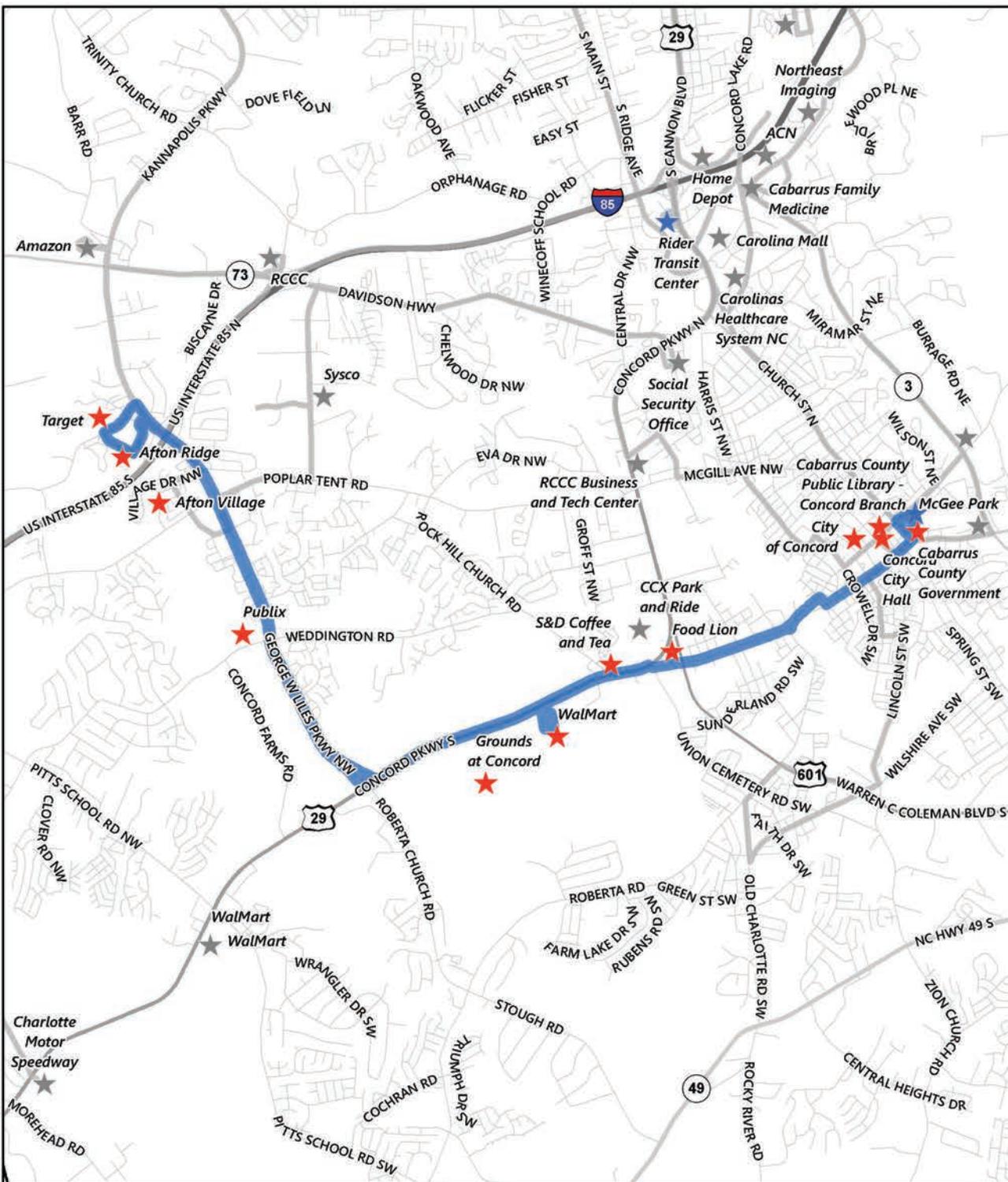
Concord Route 208

Start Phase: 3

-  Route 208
-  Activity Center
-  Transit System
-  County Boundary



ROUTE 302 - GEORGE W. LILES PARKWAY



Concord Route 302

Start Phase: 3

 Route 302

 Transit System

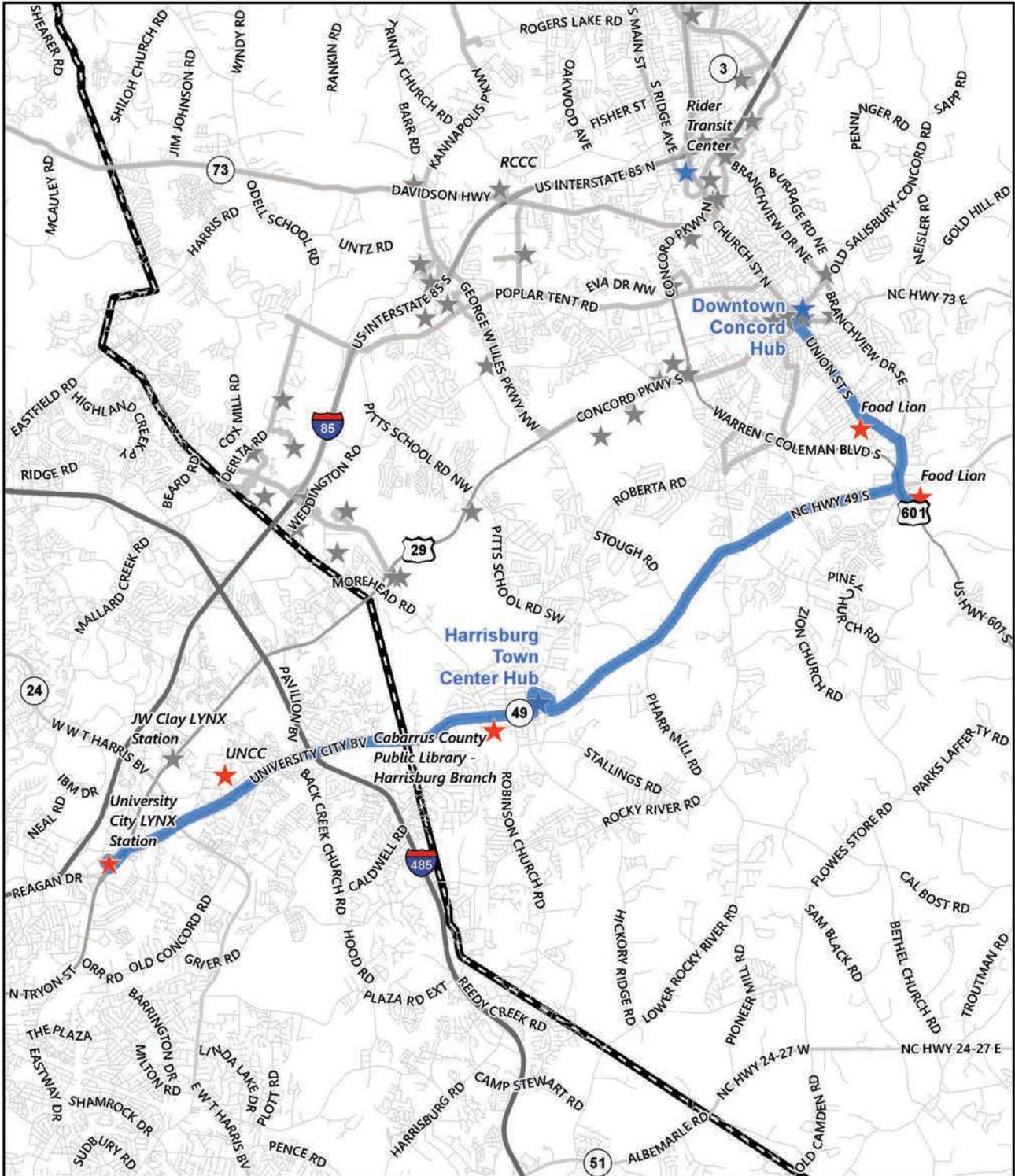
 Activity Center

 Hub

 County Boundary



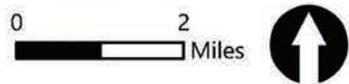
ROUTE 304 - CONNECTIONS WITH HARRISBURG



Concord/Harrisburg Route 304

Start Phase: 5

- Route 304
- Transit System
- ★ Activity Center
- ★ Hub
- County Boundary



(THIS PAGE INTENTIONALLY LEFT BLANK)